

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

MICROFILMING INFORMATION SHEET

TO: LASON

FROM: Town of Longboat Key
Town Clerk Department
501 Bay Isles Road
Longboat Key, FL 34228

DATE: 03-22-2002

SUBJECT: Microfilming

Please index the attached collection of records utilizing the following language and placing this language in the upper right hand corner of each image.

HFI DEVELOPMENT INC QUASIJUDICIAL FILE

The database should include the following fields:

QUZ059

Roll# 66

Image# 6

This collection of records should be placed on the following film type:

 16 mm

 X 35 mm

Total number of pages in this collection: 93

If there are questions regarding the collection of records that are to be microfilmed please contact Jo Ann Dunay-Mixon, Deputy Clerk Records, at 941-316-1999.

TOWN OF LONGBOAT KEY
PLANNING AND ZONING BOARD REGULAR MEETING
****TUESDAY, MAY 20, 1997 -- 9:00 A.M.****

TOWN HALL COMMISSION CHAMBERS

AGENDA

1. Swearing in of New Members
2. Roll Call
3. Election of Chairman, Vice-Chairman and Secretary
4. Approval of Minutes: April 15, 1997 Regular Meeting
 April 7, 1997 Comprehensive Plan
5. Carolyn's Retreat, 611 Linley Street (QUASI-JUDICIAL)
 a. Preliminary and Final Plat
6. HFI Developments, Inc. (QUASI-JUDICIAL)
 a. Site Plan Amendment (Wedebrook Real Estate, 6350 Gulf of Mexico Drive)
7. PUBLIC HEARING: Draft Ordinance providing for Telecommunication Antennas
8. WORKSHOP DISCUSSION: Light & Air Ordinance:
 1) Vacant PUD lots
 2) Non-conforming wide & shallow lots -
 (referred from Town Commission)
9. Old Business
10. New Business
11. Staff Report
 a. Site Plan Exemptions issued 3-18-97 through 5-12-97
12. Setting Future Meeting Dates:
 June 17, 1997: Regular Meeting
13. P&Z Board Member Comments
14. Comments from the Public and the Press
15. Adjournment

If any person decides to appeal any decision made by the Board with respect to any matter considered at this meeting, a record of the proceedings will be needed. For such purpose that person may need to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which the appeal is to be based.

MEMORANDUM

DATE: 05-14-97

TO: Planning & Zoning Board
FROM: Scott Pickett, ^{SP} Acting Planning & Zoning Director
SUBJECT: HFI DEVELOPMENTS INC.: SITE PLAN AMENDMENT APPLICATION

APPLICANT: Mr. James Layfield
HFI Developments, Inc.
6350 Gulf of Mexico Drive
Longboat Key, FL 34228

SITE LOCATION: 6350 Gulf of Mexico Drive

EXISTING ZONING: C-1, Limited Commercial District

FUTURE LAND USE
DESIGNATION: CL, Limited Commercial

EXISTING USE: One (1) office building with real estate
offices on the first floor and two (2) 880
square foot accessory dwelling units/apartments
on the second story.

REQUEST: The applicant requests site plan amendment approval to
develop a drive-thru facility exiting onto Wake Island
Road and to make changes to the approved building
addition.

BACKGROUND

Prior to the subject site plan amendment application staff had a
number of meetings with the applicant. The main focus of the
discussion at these meetings was whether the request qualified as a
site plan exemption which could be processed by staff, as opposed to
a site plan amendment which requires site plan review by the P&Z
Board and Town Commission.

After receiving a request for Site Plan Exemption approval for the
above referenced improvements, staff responded in the attached memo,
dated March 13, 1997, that the proposed revision to the approved
site plan did not constitute a minor development proposal and/or

HFI DEVELOPMENTS INC.: SITE PLAN AMENDMENT APPLICATION
05/14/97
Page 2

change to the approved site plan. Accordingly, the applicants were advised that the request could not be processed as a Site Plan Exemption, and that the request would require site plan amendment approval.

The applicant and his representatives subsequently arranged a meeting with Town staff for the purpose of reconsidering staff's determination. At that meeting staff was very concerned about the original design of the proposed drive-thru which involved 1) entering onto Wake Island Road from the parking area, 2) exiting Wake Island Road onto the drive-thru lane, and 3) exiting the drive-thru lane back onto Wake Island Road, which in effect made a portion of Wake Island Road a part of the drive-thru facility.

At the meeting, the applicant agreed to redesign the drive-thru lane so that the entire lane was located on the subject property, with only one exit onto Wake Island Road. The redesign addressed one of staff's major concerns with the proposal. Accordingly, staff agreed to reconsider its procedural determination.

The initial redesign of the drive-thru facility was successful in limiting the drive-thru to only one (1) exit onto Wake Island Road. However, the redesign raised a number of safety related issues. Due to the magnitude or scope of the safety issues, staff reaffirmed its previous determination in the attached memo dated April 9, 1997.

Finally, to assist staff in its review of the safety of the proposed drive-thru facility, the Town contracted the traffic engineering services of Post, Buckley, Schuh & Jernigan, Inc. (PBS&J). Review comments from Mr. Eric Hendra of PBS&J will be discussed later in this report and are attached for your review.

PROJECT SUMMARY

The 1.3 acre subject property is located immediately adjacent to the Laguna PUD site (see location map). In fact, the property is legally described as Parcel 1 of the Laguna at Longboat Key Subdivision, which was approved by the Town Commission on March 6, 1995. The subject property received Site Plan Approval through the adoption of Resolution 85-07.

Through the adoption of Resolution 95-21, the Town Commission approved a site plan amendment application to allow the construction of a building addition. The addition included a roofed concourse structure and a 5,124 sq. ft. office/retail facility. The applicant requests an amendment to the Site Plan to permit the development of

HFI DEVELOPMENTS INC.: SITE PLAN AMENDMENT APPLICATION
05/14/97
Page 3

a drive-thru facility exiting onto Wake Island Road and to make changes to the approved, unconstructed building addition.

PROPOSED DRIVE-THRU FACILITY

The proposed drive-thru facility is located along the north wall of the approved building addition. The drive-thru is intended for a bank tenant. However, the request is for a "general" drive-thru facility intended for the use of any permitted use in the C-1 Zoning District.

The entrance to the drive-thru is located at the northern end of the existing parking area located behind the existing building. The proposed placement of the drive-thru necessitated the removal of one (1) approved parking space. The removal of this one parking space reduces the allowable retail space in the building addition to 1,100 sq. ft. (retaining the one space would allow the applicant to have a total of 2,113 sq. ft. of approved retail uses in the building addition).

The drive-thru extends along the northern wall of the building addition. A drive-thru window is located at the northern wall to serve drive-thru patrons. The drive-thru exits onto Wake Island Road approximately one-hundred (100) feet from the intersection of Wake Island Road and Gulf of Mexico Drive. A stop sign, a two-way traffic sign and a stop bar are proposed at the intersection of the drive-thru and Wake Island Road. At the intersection of Wake Island Road and Gulf of Mexico Drive, the applicant proposes a combined back-lit street name/stop/pedestrian crossing sign, a stop bar and striping of the sidewalk.

The applicant proposes to improve the westernmost 140 feet of Wake Island Road with a new asphalt surface. The improved road surface extends to a point twenty (20) feet beyond (eastward) of the drive-thru exit onto Wake Island Road. Six (6) inch tall mountable curbing is proposed along the southern right-of-way line of Wake Island Road, from the intersection of Gulf of Mexico Drive to the drive-thru entrance. Ninety feet of curbing is proposed along the northern edge of the drive-thru lane from the drive-thru entrance to a point approximately seventy (70) feet from the drive-thru window.

In addition, four (4) foot high bollards are proposed to be located between the north wall of the approved building addition and the south side of the drive-thru lane.

Finally, the applicant proposes to install crushed shell directly east of where the proposed asphalt surface on Wake Island Road

HFI DEVELOPMENTS INC.: SITE PLAN AMENDMENT APPLICATION
05/14/97
Page 4

terminates. This shell surface realigns the improved roadway with the existing shell roadway surface.

CHANGES TO APPROVED BUILDING ADDITION

The proposed changes to the approved building addition include the following:

1. Removal of doorways from northern wall.
2. Addition of a drive-thru window on the northern wall.
3. The roofed concourse structure between the existing and approved building wings has been changed

from

a painted stucco or concrete finish on lathed steel columns or concrete columns, and beams with a tinted glass or fiberglass sandwich panel skylight and solid roof

to

a comparably designed metal column/beam frame with a flame resistant fabric or awning roof designed to separate from frame at maximum 65 MPH wind load design.

This application for Site Plan approval has been reviewed by staff for compliance with all applicable requirements of Town Code. Staff's assessment of this site plan application is provided below for your review and consideration.

STAFF ASSESSMENT

Staff's assessment of the site plan amendment application will focus on 1) safety issues related to the drive-thru facility traffic exiting onto Wake Island Road and 2) the proposed changes to the roofed concourse structure.

PROPOSED DRIVE-THRU FACILITY

Staff relied on two sections of the Zoning Code in its review of the proposed drive-thru facility. The first is Section 158.131, *Drive-in Facilities*. The proposed drive-thru facility has been determined by staff to be in compliance with the various standards contained within this section of the Zoning Code.

HFI DEVELOPMENTS INC.: SITE PLAN AMENDMENT APPLICATION
05/14/97
Page 5

Staff also reviewed the proposed drive-thru facility for compliance with Section 158.102(D) of the Zoning Code. This section provides performance standards for parking, internal circulation, and access to public or private streets. This section requires that "driveways and areas for parking and internal circulation of vehicles shall be located, designed and controlled so as to provide for safe and convenient circulation within the site and safe and convenient access from adjoining streets".

As noted above, the Town has obtained traffic engineering consultant services from Mr. Hendra of PBS&J. Mr. Hendra was directed to review the proposed drive-thru facility for compliance with Town Code Section 158.102(D). Mr. Hendra was also given an opportunity to visit the subject site. In addition, Mr. Hendra received a full copy of the subject application.

Mr. Hendra's review comments are found in the attached memos dated May 2, 1997 and May 12, 1997. To assist the Board's review of the subject application, staff has outlined the following outstanding issues and recommended improvements that have been identified by Mr. Hendra.

1. Channelization of Drive-thru Entrance

Three concerns were associated with the entrance to the drive-thru facility. The first is a concern over the protection of a vehicle parked in the space immediately south of the drive-thru entrance. This concern has been addressed through the recommended curbed separation of the subject parking space and the drive-thru entrance. Please see the attached illustration of the drive-thru facility with proposed and staff recommended improvements.

Another concern is the lack of vehicular channelization into the drive-thru entrance. The curbing recommended above, along with the proposed curbing along the north edge of the drive-thru lane provides a visually defined, channelized entrance into the drive-thru.

Finally, there was a concern that vehicles entering the drive-thru lane would overhang into the twenty (20) foot wide Wake Island Road. The median between Wake Island Road and the drive-thru lane varies in width from two (2) feet at the drive-thru entrance to six (6) feet at the drive-thru exit. Staff has assessed that a car will not overhang the two foot wide median at the drive-thru entrance under normal, reasonable traffic circulation.

HFI DEVELOPMENTS INC.: SITE PLAN AMENDMENT APPLICATION
05/14/97
Page 6

The proposed 2-6 foot separation between the roadway and the drive-thru lane represents an unique case on the Key. As a result, staff was very concerned about the separation between vehicles in the proposed drive-thru and vehicles on Wake Island Road. However, after analyzing the proposed drive-thru, staff has concluded that the design of the drive-thru meets minimal standards.

2. Turning Radius at Wake Island Road/GMD Intersection

Mr. Hendra, in his review, noted that the southern radius at the intersection of Wake Island Road and Gulf of Mexico Drive is inadequate for the anticipated traffic utilizing the facility. The applicant has submitted a FDOT connection permit for the intersection improvements. The southern radius at this intersection will be reviewed by FDOT engineers prior to the issuance of a FDOT connection permit.

3. Wake Island Road Improvements

The applicant has not proposed to improve the entire twenty (20) foot wide right-of-way of Wake Island Road. The asphalt surfaced street, in compliance with Town street construction standards, would extend from the intersection of Gulf of Mexico Drive eastward to a point twenty (20) feet east of the drive-thru exit onto Wake Island Road (see attached staff-prepared illustration).

In addition, the applicant proposes to provide a tapered shell area immediately east of where the proposed asphalt surface terminates. This shelled area provides a transition to the existing shell-surfaced roadway.

The applicant does not propose to improve the Wake Island roadway eastward of the proposed shell transition area. The roadway eastward of the shell transition area, as proposed, will remain as existing. The roadway within the Wake Island Road right-of-way in this area is less than twenty (20) feet wide, with a minimum width of fourteen (14) feet.

This portion of the right-of-way (between the terminal point of the asphalt-surfaced roadway and the entrance/exit for the easternmost parking area) will need to accommodate traffic generated from the subject property, as well as the Town Public Works/PZ&B complex. In accordance with Town Code section 158.102(D), staff has determined that this portion of the Wake Island Road right-of-way will need to be improved with a twenty

HFI DEVELOPMENTS INC.: SITE PLAN AMENDMENT APPLICATION
05/14/97
Page 7

(20) foot wide, shell-surfaced roadway so as to safely accommodate anticipated traffic.

The shell roadway improvement is recommended at this time because at the time the building addition was approved, future uses had not been specifically identified. In other words, the Town had no knowledge of future tenants in the approved building addition. Future tenants would be those uses permitted in the C-1 Zoning District.

Given the uncertainty of future conditions on the site, it would have been pre-mature and speculative of the town to require specific improvements to Wake Island Road. With a specific development proposal now submitted to the Town, staff have been able to evaluate the impacts of the proposal on Wake Island Road and determine that the above recommended improvements to Wake Island Road are necessary to provide safe and convenient traffic circulation.

4. Internal Circulation: Signs and/or Painted Directions

Based on input from staff, the applicant has proposed a number of traffic information improvements to facilitate safe and convenient circulation within the subject property as well as safe and convenient traffic movements exiting the proposed drive-thru facility.

The attached staff-prepared illustration of the proposed drive-thru facility shows several additional recommended traffic information/control improvements that will further facilitate safe and convenient traffic circulation within the subject property as well as safe and convenient ingress and egress into/out of the subject property from Wake Island Road. Recommended improvements include stop signs at the intersection of Wake Island Road and the exits of both rear parking areas, revised drive-thru directional painted information within the parking area aisle, and additional two-way directional arrows painted within a parking area aisle.

5. Turning Radius at Existing Driveway onto Gulf of Mexico Drive

In his review, Mr. Hendra also noted that the southern radius of the property's driveway entrance onto Gulf of Mexico Drive is inadequate due to the geometry of the intersection. Given the increase in traffic volume at the subject property with the proposed drive-thru facility, staff would have no objection,

HFI DEVELOPMENTS INC.: SITE PLAN AMENDMENT APPLICATION
05/14/97
Page 8

should the applicant be interested, to the modification of this radius.

PROPOSED CHANGES TO ROOFED CONCOURSE STRUCTURE

As you will recall, the previous site plan amendment for the subject site was considered a building addition because the approved concourse structure structurally and architecturally connected the two wings of the building. This connection of the two wings was required by Chapter 157 of the Town Code which stipulates that there be no more than one principal structure on any one lot of record.

In reviewing the proposed changes to the concourse structure, staff assessed whether the structural and architectural connection between the wings has been retained. Staff has determined that the proposed concourse structural will provide sufficient structural connection between the two wings that is similar to what was originally approved by the Town.

However, the building addition was approved as a single building including both wings and the concourse structure. Accordingly, the approved concourse structure was required to comply with Town 110 mph wind load requirements. As such, staff recommends that the entire concourse structure, including its roof, shall be designed to a minimum wind load of 110 mph.

In assessing the architectural connection provided by the concourse structure, it is important to note the main changes to the concourse structure involve changes in material and the reduced size of structural components of the concourse structure. The roof of the concourse structure has been changed from a "solid roof" to a fabric roof. The painted stucco or cement surface of the columns and beams have been changed to a metal surface. Finally, the width of the columns has been reduced from 12" wide to 8" wide.

Staff has assessed that compared to the approved concourse structure, the proposed changes to the concourse reduces the architectural connection of the two building wings. However, in reviewing this aspect of the application, the Town needs to assess whether a minimum acceptable architectural connection is provided. Upon review of the proposed concourse structure, staff has determined that the minimum required architectural connection between the two building wings has been retained. Accordingly, staff has NO OBJECTION to the proposed revision to the concourse structure provided that the fabric roof is designed in compliance with Town wind load requirements.

HFI DEVELOPMENTS INC.: SITE PLAN AMENDMENT APPLICATION
05/14/97
Page 9

As per Town Code Chapter 158.103, the granting or denial of applications for site plan approval by written resolution shall include not only conclusions, but also findings of fact related to the specific proposal, and shall set forth the reasons for the granting of approval, with or without changes or special conditions, or for the disapproval. The resolution shall set forth with particularity in what respects the plan would or would not be in the public interest, including but not limited to findings of fact and conclusions on the following.

To facilitate discussions by the Planning and Zoning Board, the following findings and conclusions have been prepared by staff for your review and consideration:

FINDINGS OF FACT/CONCLUSIONS

- A. The site plan, with the changes recommended by staff, IS consistent with the Comprehensive Plan and the purpose and intent of the zoning district in which it is located.
- B. The site plan, with the changes recommended by staff, IS in conformance with all applicable regulations of the zoning district in which it is located.
- C. The site plan, with the changes recommended by staff, IS in conformance with the Town's subdivision regulations, Chapter 157, and all other applicable Town Code requirements, including the design, adequacy, and construction of streets, drainage, utility facilities, and other essential services.
- D. The site plan, with the changes recommended by staff, IS consistent with good design standards in respect to all external relationships, including but not limited to relationship to adjoining properties; internal circulation, both vehicular and pedestrian; disposition and use of open space, provision of screening and buffering, and preservation of existing natural features, including trees; size and apparent bulk of structures; and building arrangements both between buildings in the proposed development and those adjoining the site.
- E. The site plan, with the changes recommended by staff, IS in conformance with Town policy in respect to sufficiency of ownership.

HFI DEVELOPMENTS INC.: SITE PLAN AMENDMENT APPLICATION
05/14/97
Page 10

STAFF RECOMMENDATION

Based upon staff's assessment, all procedural and substantive requirements of Town Code regarding the site plan amendment application have been satisfied, and subject to the conditions contained in Exhibit "A" of the attached draft Resolution, staff would recommend APPROVAL of the HFI Developments, Inc. Site Plan Amendment Application.

Attached, please find a copy of the site plan amendment application and support documentation upon which the staff assessment has been based. If you should have any questions, or desire any additional information, please do not hesitate to contact the Planning, Zoning & Building Department.

/sp

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

TOWN OF LONGBOAT KEY
PLANNING, ZONING & BUILDING DEPARTMENT
610 General Harris Road Longboat Key, Florida 34228
Phone (941) 316-1966 Fax (941) 316-1970

Date: March 13, 1997

Project: Wedebrook Building Addition

Applicant: William Houghton, Landry & Esber

Request: Site Plan Exemption Approval for Development of Bank Drive-thru Connecting onto Wake Island Alley and Revision to Approved Roofed Concourse Structure

Reviewer: Scott Pickett, ^{SP} Planner

Staff has reviewed the above referenced request, submitted March 5, 1997, and determined that the proposed revision to the approved site plan is not a minor development proposal and/or change of the approved site. As such, staff can not process your request as a site plan exemption application.

Accordingly, the proposed revisions to the approved site plan will need to be processed as a site plan amendment application in accordance with Town Code Section 158.099. The submission requirements for a site plan amendment application are specified in Town Code Section 158.097.

Please be aware that the "annual site and development plan season" for the Planning & Zoning Board and Town Commission includes the months September through June of each year. As such, both bodies can not review or act on any site plan amendment application after the end of June until September of this year.

The completeness deadline for the May 20, 1997 Planning & Zoning Board regular meeting is April 22, 1997. Staff will work with the applicant to facilitate an expeditious review and processing of the required site plan amendment application for the request described above.

cc: Jim Layfield
Michael Furen
Mark Wickersham
Building File

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

TOWN OF LONGBOAT KEY
PLANNING, ZONING & BUILDING DEPARTMENT
610 General Harris Road Longboat Key, Florida 34228
Phone (941) 316-1966 Fax (941) 316-1970

Date: April 9, 1997

Project: Wedebrook Building Addition (Approved by Resolution 95-21)
6350 Gulf of Mexico Drive

Applicant: William Houghton, Landry & Esber, Agent

Request: Site Plan Exemption Approval for Development of Bank Drive-thru Connecting onto Wake Island Road and Revisions to Approved Roofed Concourse Structure and Building Addition

Reviewer: Scott Pickett,^{SP} Acting Planning & Zoning Director

As you will recall, staff responded to the above referenced request in the attached memorandum dated March 13, 1997. In the attached memo, staff determined that the proposed revisions to the approved site plan did not represent a minor development proposal and/or change of the approved site plan. Staff also determined that the request could not be processed as a site plan exemption, but would require a site plan amendment application in accordance with Town Code Section 158.099.

Staff met with several representatives for the subject project in an attempt to address their objections to staff's determination. At that meeting, in an attempt to minimize the scope of the development proposal, an alternative bank drive-thru design was identified. On April 3, 1997 staff received a revised site plan with the drive-thru contained within, the subject property. To date, staff has not received building plans showing proposed revisions to the approved building addition.

Staff has reviewed the revised site plan and reaffirms its prior determination that the proposed revisions to the approved site plan do not represent a minor development proposal and/or change of the approved site plan. Aspects of the proposal which are considered major changes to the approved site plan include the use of Wake Island Road as an exit for the proposed bank drive-thru and the reported changes to the concourse roof separating the existing and proposed wings of the building.

Staff has determined that the proposed revisions to the approved site plan will need to be processed as a site plan amendment application in accordance with Town Code Section 158.099. The submission requirements for a site plan amendment application are specified in Town Code Section 158.097.

Please be aware that the "annual site and development plan season" for the Planning & Zoning Board and Town Commission includes the months September through June of each year. As such, both bodies can not review or act on

Site Plan Exemption Approval Request for Development of Bank Drive-thru
Connecting onto Wake Island Road and Revisions to Approved Roofed Concourse
Structure and Building Addition
April 9, 1997
Page 2

any site plan amendment application after the end of June until September
of this year.

The completeness deadline for the May 20, 1997 Planning & Zoning Board
regular meeting is April 22, 1997. Staff will work with the applicant to
facilitate an expeditious review and processing of the required site plan
amendment application for the request described above.

cc: Jim Layfield
Michael Furen
Mark Wickersham
Building File



ENGINEERING
PLANNING

May 2, 1997

Mr. Scott Pickett,
Acting Planning & Zoning Director
Town of Longboat Key, Florida
610 General Harris Drive
Longboat Key, FL 34228

RE: HFI, Inc. Development Site Plan Review

Dear Mr. Pickett:

Per your request, we have reviewed the site plan amendment submittal of the above referenced project for safety and design with accordance to the Longboat Key Zoning Code. Our review has the following items and recommendations to be addressed by applicant:

1. Both entrances to Gulf of Mexico Drive do not appear to provide adequate turning radiuses for northbound vehicles entering. Entering vehicles will cross the centerline creating an unsafe condition for both areas entrances.

Recommendation: Increase turning radius for both entrances to provide safe traffic access.

2. Traffic flows from present use to the expanded facility will be significantly increased. Vehicles will enter the site from the south entrance on Gulf of Mexico Drive and exit onto Wake Island Road. This increased volume onto Wake Island Road will degrade the existing shell surface course and will require regular maintenance.

Recommendation: Extend Wake Island Road pavement full width (20 feet) to last driveway east.

3. Traffic flow arrows are not clearly identified to be painted on the pavement. Arrows ambiguously suggests one-way traffic flow condition and the drive-thru note, "TO BANK DRIVE THRU" does not address the incoming traffic from Gulf of Mexico Drive to drive-thru.

Recommendation: Add two-way traffic flow arrows to encourage traffic to exit through site entrance as well as to Wake Island Road. Label the arrows as painted and rotate drive-thru note.

4. The eastern parking entrance on Wake Island Road does not provide facilities for storm water flows across or under the entrance.

May 2, 1997
Mr. Scott Pickett
RE: HFI, Inc. Development Site Plan review
Page 2

Recommendation: Add culvert or provide grading of driveway accommodating the stormwater flows.

5. The drive-thru entrance does not provide adequate protection between adjacent parking stall and to the future Wake Island Road extension. Drive-thru vehicles will have a maximum overhang of approximately three feet that will extend into the right-of-way of Wake Island Road and the oncoming future traffic condition. Curbing has been provided for Wake Island Road vehicles and not for the drive-thru vehicles. Supporting documents discuss the use of curbing to channel vehicles on roadways. The unsafe condition will be from the overhang of vehicles in the drive-thru. No channelization control has been provided for the drive-thru vehicles.

Recommendation: Provide median and curbing on both sides of drive-thru with sufficient width to accommodate large vehicle access and provide protection of the parked vehicle and oncoming traffic on Wake Island Road. This layout will require eliminating one parking stall adjacent to the drive-thru. Re-orientation of the parking lot can make up for the loss based upon preliminary layout sketches.

If you should have any further questions regarding this matter, please feel free to call me at your earliest convenience.

Sincerely,

POST, BUCKLEY, SCHUH & JERNIGAN, INC.



Eric J. Hendra, P.E.
Sr. Project Engineer

cc: Mr. Pete Putman

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

PBS&J-SARASOTA

ID:8139511477

MAY 05 '97

10:30 No.002 P.01

PBS&J POST,
BUCKLEY,
SCHUH &
JERNIGAN, INC.

COMP. BY: ESH

CHK. BY: CBP

DATE: 5/2/97

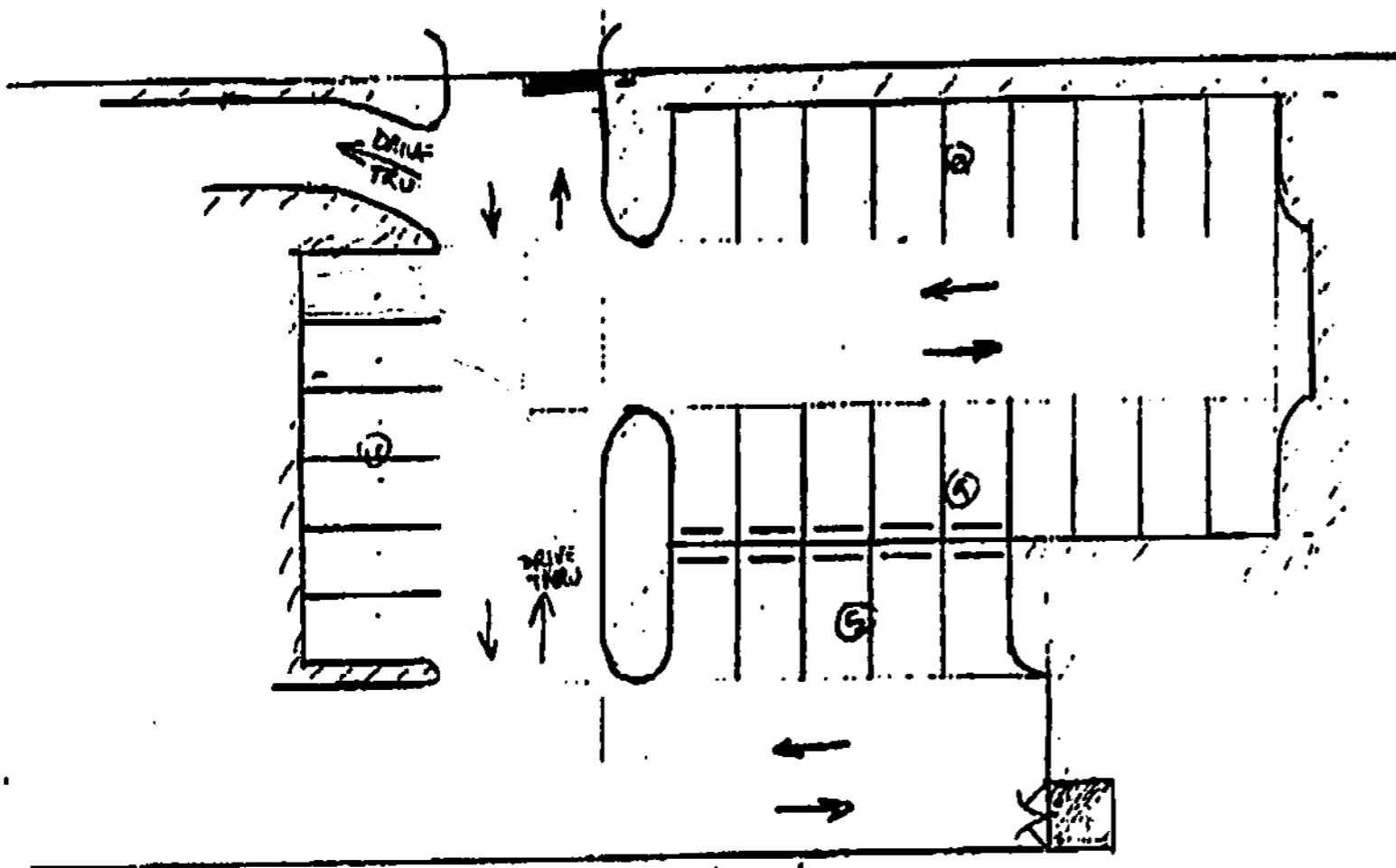
SHEET NO.: _____

JOB NO.: _____

Subject: HFI DEVELOPMENT INC

NEW PARKING DESIGN

1"=30'



9
9
5
6

29
+ 4 FRONT

33 SPACES

Project # 95
For Note
To **SCOTT PUCKETT**
Fax 96-1970
From **ERIC HEWDE**
Phone 951 1477

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE



ENGINEERING
PLANNING

May 12, 1997

Mr. Scott Pickett,
Acting Planning & Zoning Director
Town of Longboat Key, Florida
610 General Harris Drive
Longboat Key, FL 34228

RE: HFI, Inc. Development Site Plan Review

Dear Mr. Pickett:

Per our phone discussions on Friday May 9, 1997, the revised plans have addressed the majority of the review items and are acceptable based upon the conditions set forth in my May 2, 1997 letter and the additional conditions below:

1. The plans have provided an 85± foot traffic separator for the north side of the drive-thru. The adjacent parking stall is still unprotected from the drive-thru. A raised curb along the north side of the stall should be constructed for increased vehicle protection.
2. The inside radiuses for the two entrances to Gulf of Mexico Drive have not been increased. The 35 foot radius on Wake Island Road will remain off Gulf of Mexico Drive and compound to a minimum of 18 feet on the inside corner radius. This radius will accommodate only large vehicles and not for vehicles such as trucks. Based upon conversations with the design engineer, they have not been revised because of an approval of the plans by your staff. This matter will need to be addressed by your staff.
3. The extension of the pavement to just past the drive-thru is acceptable since the amended plans are concerned with only the drive-thru facility. No further extension will be required.

If you should have any further questions regarding this matter, please feel free to call me at your earliest convenience.

Sincerely,

POST, BUCKLEY, SCHUH & JERNIGAN, INC.

Eric J. Hendra, P.E.
Sr. Project Engineer

cc: Mr. Pete Putman

fcivil111997docs\longk2.doc

800ew.12

D4 - 05/16/97

RESOLUTION 97-14

A RESOLUTION OF THE TOWN OF LONGBOAT KEY, FLORIDA, AMENDING RESOLUTION 95-21, APPROVING A SITE PLAN AMENDMENT FOR HFI DEVELOPMENTS, INC. (a/k/a Wedebrook Realty) LOCATED AT 6350 GULF OF MEXICO DRIVE, TO ADD A DRIVE-THRU FACILITY EXITING ONTO WAKE ISLAND ROAD AND MAKE CHANGES TO THE APPROVED BUILDING ADDITION; PROVIDING FOR AN EFFECTIVE DATE.

WHEREAS, the site plan for HFI DEVELOPMENTS, INC. (a/k/a Wedebrook Realty) located at 6350 Gulf of Mexico Drive, Longboat Key, Florida, was approved by the Town by Resolution 85-7, adopted February 4, 1985; and

WHEREAS, Resolution 85-7 was subsequently amended by Resolution 95-21 and 96-22; and

WHEREAS, HFI Developments, Inc., has requested a site plan amendment to add a drive-thru facility for permitted C-1 uses exiting onto Wake Island Road and make changes to the approved building addition; and

WHEREAS, the Planning and Zoning Official has, in a timely fashion, accepted the Application and referred same to the Planning and Zoning Board along with the support documentation and staff recommendations; and

WHEREAS, the Planning and Zoning Board has reviewed the Application and has recommended to the Town Commission along with their findings that the proposed development be approved with conditions; and

WHEREAS, the Town Commission makes these conclusions and findings of fact:

- (a) The plan is consistent with the comprehensive plan and the purpose and intent of the zoning district in which it is located.
- (b) The plan is in conformance with all applicable regulations of the zoning district in which it is located.

RESOLUTION 97-14 (CONT)
D4 - 05/16/97

- (c) The plan is in conformance with the Town's subdivision regulations, Chapter 157, and all other applicable Town requirements, including the design, adequacy, utility facilities, and other essential services.
- (d) The plan is consistent with good design standards in respect to all external relationships, including but not limited to relationship to adjoining properties; internal circulation, both vehicular and pedestrian; disposition and use of open space, provision of screening and buffering, and preservation of existing natural features, including trees; size and apparent bulk of structures; and building arrangements both between buildings in the proposed development and those adjoining the site.
- (e) The plan is in conformance with Town policy in respect to sufficiency of ownership, guarantees for completion of all required improvements, and, if private, the guarantees for continued maintenance.

NOW, THEREFORE, BE IT RESOLVED BY THE TOWN OF LONGBOAT KEY, THAT:

Section 1. The site plan amendment for the HFI DEVELOPMENTS, INC., 6350 Gulf of Mexico Drive, Longboat Key, Florida 34228 be and is hereby approved subject to the conditions attached hereto marked "Conditions Requisite for Approval", HFI Developments, Inc., 6350 Gulf of Mexico Drive, Longboat Key, Florida 34228, and dated concurrently with this Resolution.

Section 2. This Resolution shall become effective immediately upon adoption.

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

RESOLUTION 97-14 (CONT)
D4 - 05/16/97

ADOPTED at a meeting of the Town Commission of the Town
of Longboat Key on the _____ day of _____,
1997.

Mayor

ATTEST:

Town Clerk

Attachment: Exhibit "A"
Conditions for Approval

RESOLUTION 97-14 (CONT)
D4 - 05/16/97

EXHIBIT "A"
CONDITIONS REQUISITE FOR APPROVAL

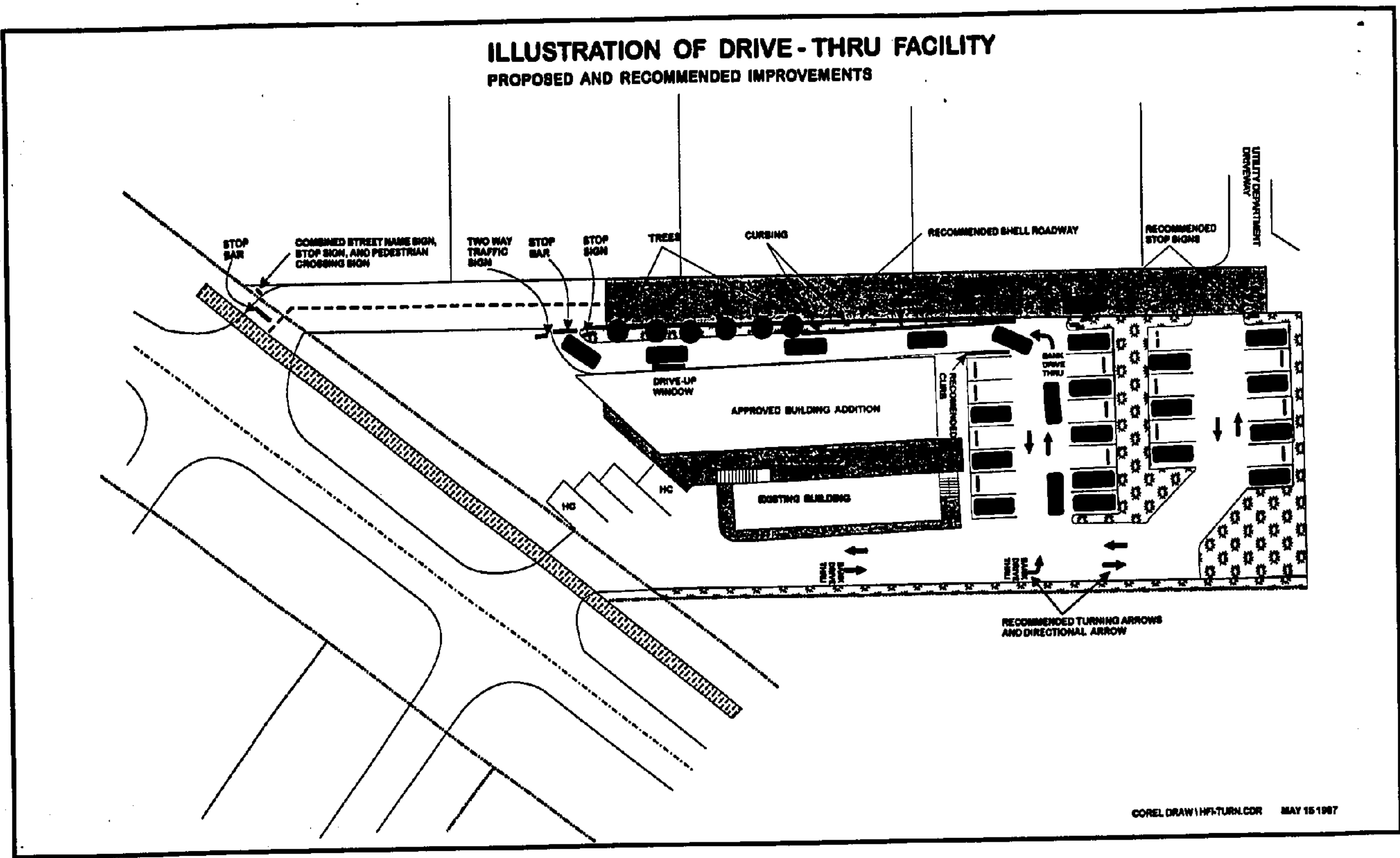
HFI DEVELOPMENTS, INC.

- 1) The provisions of the site plan application for the subject property dated April 22, 1997, received on May 6, 1997, shall be complied with unless waived or modified by the above conditions or by written agreement between the Town and the applicant or amended pursuant to Code. Any and all improvements shall comply with C-1 provisions of Town Code.
- 2) Prior to the issuance of any building permit, all applications for permits submitted to any outside permitting agency, and all applicable permits received from such agencies shall be submitted to the Planning, Zoning & Building Department.
- 3) All on-site infrastructure, including but not limited to, utilities and landscaping, stormwater systems and grading shall be completed prior to issuance of any certificate of completion.
- 4) Approval of utilities, stormwater system and all site work, including all necessary off-site improvements, and the posting of a satisfactory performance bond must be received from the Public Works Department prior to the commencement of any work.
- 5) All utilities shall be located underground.
- 6) A construction fence shall be provided to secure the construction site.
- 7) The stormwater management system shall be designed and maintained in perpetuity in accordance with the provisions of the SWFWMD and the Manatee County Mosquito Control Department.
- 8) Approval of the proposed site plan shall be subject to payment of all staff review charges.

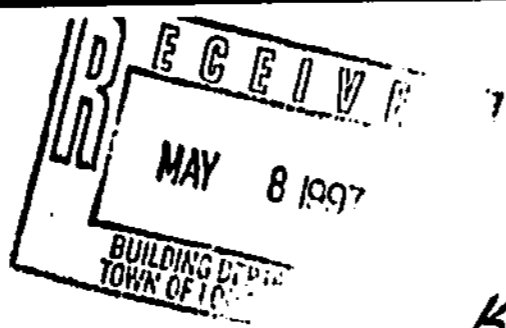
RESOLUTION 97-14 (CONT)
D5 - 05/16/97

- 9) The slope of all graded areas on the site shall not exceed a ratio of 4:1 (four (4) feet horizontal for every one (1) foot vertical). Prior to the issuance of any certificate of completion, the applicant shall submit an "as built" grading plan to and approved by the Town Planning, Zoning & Building Department.
- 10) Native and drought resistant plant species shall be used in the buffer and other common areas to reduce water requirements. No more than 25% of the site may be planted in sod or plant species that are not drought resistant.
- 11) Parking of construction-related vehicles shall be prohibited along Gulf of Mexico Drive.
- 12) The applicant shall construct a twenty (20) foot wide shell roadway, in compliance with Town street construction standards, within the right-of-way of Wake Island Road, between the eastern-most edge of the proposed asphalt-surfaced street and ten (10) foot past the exit/entrance of the eastern-most approved parking area.
- 13) The combined backlit street name sign, stop sign and pedestrian crossing sign shall be of the same construction as the Town signs located at the intersections of Gulf of Mexico Drive and General Harris Street/Lyons Lane.
- 14) The applicant shall be required to construct those proposed and staff recommended improvements indicated on the attached site plan drawing, titled "Illustration of Drive-Thru Facility", dated May 15, 1997.
- 15) The entire concourse structure, including its roof, shall be designed to a minimum wind load of 110 mph.

ILLUSTRATION OF DRIVE-THRU FACILITY PROPOSED AND RECOMMENDED IMPROVEMENTS



HFI DEVELOPMENT INC
QUASIJUDICIAL FILE



5/8/97

LONGBOAT KEY
PLANNING + ZONING BOARD
CHAIRMAN HARRY ROSENBERG

KARL REINFANDT
6391 GULF OF MEX DR.
LONGBOAT KEY FL 3422

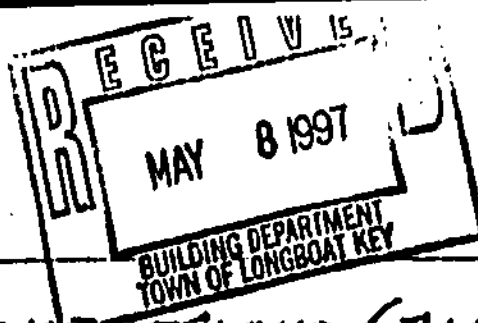
MEMBERS OF THE BOARD,

YESTERDAY I RECEIVED A CERTIFIED LETTER FROM
WEDERBROIC REAL ESTATE (HFI DEVELOPMENT)
REFERENCE TO A SITE PLAN AMENDMENT REQUEST.

THE LETTER INDICATED I COULD GET FURTHER
INFORMATION FROM THE PLANNING AND ZONING DEPT.
THE ZONING DEPT, FIRST THING THIS MORNING, ADVISED
ME THAT THE REASON FOR THE AMENDMENT REQUEST
WAS TO ALLOW A DRIVE THRU FACILITY WHICH WOULD EXIT
ONTO WAKE ISLAND ROAD (ALLEY) THEN ONTO GULF OF
MEXICO DR DIRECTLY ACROSS FROM GULFSIDE ROAD
INTERSECTION.

I AM STRONGLY OPPOSED TO THIS BEING ALLOWED
FOR THREE REASONS, NUMBER 1, AND PROBABLY THE
MOST IMPORTANT IS THE IMPACT, HAZARD TO PEDESTRIAN
SAFETY AS WELL AS VEHICULAR, THIS WOULD ONLY
INCREASE WHAT IS ALREADY A DANGEROUS SITUATION.
IT IS NOW ALMOST IMPOSSIBLE TO CROSS GULF OF MEXICO
DRIVE, PEDESTRIANS GOING TO THE BEACH FROM THEIR
SURROUNDING HOUSES, ESPECIALLY IN SEASON FIND IT
ALMOST IMPOSSIBLE. ADDING MORE TRAFFIC BY ALLOWING
A POSSIBLE HEAVY FLOW OF TRAFFIC TO EXIT WAKE ISLAND
ALLEY WOULD CREATE MORE THAN A REASONABLE SAFETY
HAZARD. IF THERE IS SUCH A THING!!

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE



5/8/97

NUMBER 2 WAKE ISLAND (ALLEY) IS NOT
A STREET, IT IS NOT WIDE ENOUGH LEGALLY TO BE
ACCEPT^{ED} AS A STREET, IT IS CONSIDERED AN
ALLEY WITH MINIMUM FLOW OF TRAFFIC EXPECTED

NUMBER 3 WE SHOULD NOT ALLOW ONE PROPERTY OWNER
DEVELOPER WITH PERSONAL GAIN AS THEIR PRIMARY MOTIVE TO
DEVELOPE A SITE PLAN WITH FLAGRANT DISREGARD FOR THE
SAFETY OF THE COMMUNITY THERE ARE OTHER PROPERTIES ADJACENT
TO WAKE ISLAND ALLEY CURRENTLY BEING ADVERTIZED FOR
SALE BY WEDERBROCK, IF WE ALLOW HFI DEVELOPERS THEIR
SITE PLAN AMENDMENT THEN WHAT WILL HAPPEN EVENTUALLY WHEN
THESE OTHER (4) ADJACENT WAKE ISLAND ALLEY PROPERTIES HAVE A
SIMILAR OR EVEN SAME REQUEST

I DON'T THINK THE SITE PLAN AMENDMENT SHOULD BE ALLOWED,
IT WAS ALREADY A MISTAKE TO ALLOW WEDERBROCK'S BEAR PARKING AREA
TO EXIT ON TO WAKE ISLAND ALLEY, INCREASING TRAFFIC A DRIVE THRU
IS NOT NECESSARY TO CONDUCT NORMAL BUSINESS, PEOPLE CAN TAKE
TIME TO GET OUT OF THEIR VEHICLES AND THEN ENTER THE
ESTABLISHMENT IN QUESTION. THIS HELPS AT LEAST TO CONTROL
THE ALREADY HEAVY FLOW OF TRAFFIC ON OUR BEAUTIFUL ISLAND.

I PLAN TO ATTEND THE PUBLIC HEARING ON THIS MATTER,
HOWEVER SHOULD I FOR SOME REASON BE UNABLE TO DO SO, I WOULD THEN
RESPECTFULLY REQUEST THAT A MEMBER OF THE ZONING BOARD RETURN
MY LETTER INTO THE MINUTES OF THE MEETING

THANK YOU IN ADVANCE FOR YOUR TIME AND EFFORT.

SINCERELY
Ed R. Jones

HFI DEVELOPMENTS, INC.

**SITE PLAN AMENDMENT
DEVELOPMENT BOOK**

THIS APPLICATION FOR SITE PLAN AMENDMENT IS BEING FILED BY THE APPLICANT SOLELY AND EXCLUSIVELY TO EXHAUST ALL OF THE APPLICANT'S ADMINISTRATIVE REMEDIES AND TO OBTAIN A FINAL DECISION BY THE TOWN OF LONGBOAT KEY, FLORIDA, ON AND WITH RESPECT TO THE APPLICANT'S LAND USE AND DEVELOPMENT REQUEST. THIS APPLICATION IS BEING FILED BY THE APPLICANT WITH AN ABSOLUTE AND COMPLETE RESERVATION OF ALL OF THE APPLICANT'S RIGHTS, REMEDIES, DEFENSES, CLAIMS AND CAUSES OF ACTION.

APRIL 22, 1997

INDEX

Exhibit A-1	General Information
Exhibit A-2	Letter to Owner
Exhibit A-3	Town of Longboat Key Application for Site Plan Review
Exhibit A-4	Application for Site Plan Approval - 158.097 Performance Standards for Site & Development Plans - 158.102
Exhibit A-5	Letter to Town of Longboat Key
Exhibit A-6	Drainage Analysis
Exhibit A-7	Traffic Impact Analysis Post, Buckley, Shuh & Jernigan, Inc. Site Plan Review
Exhibit B-1	Site Plans (attached)
Exhibit B-2	No Permits are required from D.E.P.
Exhibit B-3	SWFWMD Permit Modification
Exhibit B-4	FDOT Driveway Connection Application
Exhibit C-1	Architect's Plans (attached)
Exhibit D-1	Schedule of Construction

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

EXHIBIT A-1
GENERAL INFORMATION

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

ICARD, MERRILL, CULLIS, TIMM,
FUREN & GINSBURG, P.A.

ATTORNEYS AND COUNSELLORS
2033 MAIN STREET, SUITE 600
SARASOTA, FLORIDA 34237
FACSIMILE (941) 366-6384
TELEPHONE (941) 366-8100

TAMPA TELEPHONE
(813) 221-2100
REPLY TO:
P.O. BOX 4195
SARASOTA, FLORIDA 34230

MICHAEL J. FUREN

April 18, 1997

Mr. Scott Pickett, Acting Planning & Zoning Official
Town of Longboat Key
610 General Harris Street
Longboat Key, FL 34228

Re: Amendment to Approved Site Plan for Wedebrook Building
Addition at 6350 Gulf of Mexico Drive, Longboat Key,
Florida, to Reflect Minor Development Changes

Dear Scott:

As you are aware, our firm represents HFI Developments, Inc., a Florida corporation and the defacto owner of the Wedebrook Real Estate Company property located at the above-referenced address by virtue of a long-term lease of this property.

As you are also aware, our client has proposed two (2) minor changes or revisions to the approved site plan for the building addition at this location, i.e., a drive-thru facility for a bank tenant and a change in the materials in the "roof over" connection between the existing building and the building addition.

You are also aware that we and our client believe that these proposed changes in the approved site plan for the building addition are clearly minor development changes that you should have properly approved as "site plan exemptions" pursuant to Section 158.100 of the Town's Zoning Code.

Our client totally disagrees with your determination that the two (2) proposed minor development changes to the approved site plan are required to undergo the full site plan amendment review process before the Town's Planning & Zoning Board and the Town Commission. This process, under the circumstances, is clearly unnecessary pursuant to the Town's Zoning Code, imposes unnecessary costs and expenses on our client, unjustifiably delays our client's building addition and may or probably will result in our client losing a lease with a bank tenant for the building addition.

April 18, 1997
Page 2

On March 13, 1997, despite prior contrary indications given by you to our client and its consultants, you made a preliminary determination that the two (2) minor revisions to our client's approved site plan could not be approved as site plan exemptions.

On March 21, 1997, we met with you, the Assistant Town Manager and the Town Attorney at Town Hall to discuss the matter and to find a way to resolve the dispute. We pointed out during the meeting many instances where the Town had previously approved as site plan exemptions development revisions and changes greater in scope than those proposed by our client.

Later that day we were advised that the two (2) proposed site plan changes would be approved as site plan exemptions if our client would agree (a) to eliminate the drive-thru entrance from Wake Island Road and keep the bank drive-thru traffic within the property until it exited onto Wake Island Road; (b) to pave Wake Island Road from Gulf of Mexico Drive to the drive-thru exit; and (c) install appropriate traffic control signage and devices. We were also specifically advised that the proposed change in the materials of the "roof over" structure was no longer an issue.

Our client, relying on this proposal and at considerable cost and expense, immediately had their consultants meet with you and other staff members and revised the site plan consistent with the proposal.

On April 9, 1997, the day before the revised site plan materials were going to be filed by our client's consultants with you, I received an unexpected fax from you in which you ignored all prior discussions and the compromise agreement and determined that the two (2) minor site plan changes proposed by our client would have to undergo the time consuming and expensive full site plan review process.

You are aware that we have appealed your determination that these minor development changes to our client's approved site plan cannot be approved by you as site plan exemptions pursuant to Section 158.100 of the Town's Zoning Code. This appeal is pending before the Town's Zoning Board of Adjustment.

Our client is, therefore, filing this application for approval of an amendment to the approved site plan for the Wedebrook building addition solely and exclusively to exhaust it's administrative remedies, to obtain a final decision by the Town on its proposed minor changes to the approved site plan for the Wedebrook building addition and with a full and complete reservation of our client's rights, remedies, defenses, claims and causes of action with respect to this dispute.

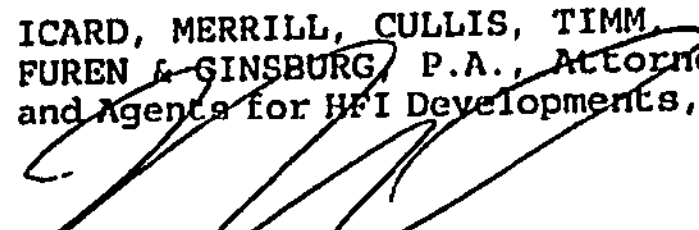
April 18, 1997
Page 3

Please call me if you should have any questions concerning this conditional, limited and qualified application for a site plan amendment filed by and in behalf of our client.

Also, please call me immediately if you should require any additional information or materials. Thank you for your prompt attention to this application.

Sincerely,

ICARD, MERRILL, CULLIS, TIMM
FUREN & GINSBURG, P.A., Attorneys
and Agents for HFI Developments, Inc.


Michael J. Furen

MJF:jwc
Enclosure

cc: Mr. Jim Layfield
Mr. Griff H. Roberts, Town Manager
Mr. Bruce St. Denis, Assistant Town Manager
David P. Persson, Esq., Town Attorney

EXHIBIT A-1
GENERAL INFORMATION

1. **NAME:** HFI Developments, Inc.
2. **LESSEE:** Mr. James Layfield
6350 Gulf Of Mexico Drive
Longboat Key, FL 34228
(941) 383-5543
3. **ENGINEER/SURVEYOR:** Landry & Esber
5940 Palmer Blvd.
Sarasota, FL 34232
(941) 379-8831
4. **ARCHITECT:** Skirball Group, Inc.
1509 S. Orange Ave.
Sarasota, FL 34239
(941) 951-1777
5. **WASTEWATER SYSTEM:** Town of Longboat Key and
Manatee County Utility Systems
6. **WATER DISTRIBUTION SYSTEM:** Town of Longboat Key and
Manatee County Utility Systems
7. **DRAINAGE SYSTEM:** Surface water runoff to swales, retention and
discharge to Sarasota Bay.
8. All construction in accordance with applicable provisions of the Town of Longboat Key Tree
Ordinance.
9. **Zoning:** C-1
10. **PROPOSED:** Office/Retail Building

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

EXHIBIT A-2
LETTER TO OWNER

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

LANDRY & ESBER

Engineers • Surveyors

April 22, 1997

Mr. James Layfield
HFI Developments, Inc.
6350 Gulf Of Mexico Drive
Longboat Key, Florida 34228

RE: HFI Developments, Inc.

Dear Mr. Layfield:

Pursuant to your request, we have prepared an amended site plan for the office building. We have enclosed a copy of the plans and development book for your records. This plan set and book will be transmitted to the Town of Longboat Key for their review and approval.


The plan set includes the following drawings:

	<u>Description</u>	<u>Sheet Number</u>
A.	<i>Site Amendment Plan:</i>	
	Cover Sheet	D-1
	Existing Conditions	D-2
	Aerial Photo	D-3
	Site Plan Amendment	D-4
	Site Data	D-5
	Tree Location	D-6
	Landscape & Stabilization Plan	D-7

If you have any questions concerning this submittal, please do not hesitate to contact us.

Sincerely,

LANDRY & ESBER


William B. Houghton, P.E.
Vice President

Enclosures

C:\WPWIN60\WPDOCS\B01-B025\B02502\LAYFIEL2 LTR April 15, 1997 (B02502)

Fax (941)379-0556 • 5940 Palmer Blvd. • Sarasota, FL 34232 • (941)379-8831

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

EXHIBIT A-3

**TOWN OF LONGBOAT KEY
APPLICATION FOR SITE PLAN REVIEW**

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

TOWN OF LONGBOAT KEY, FLORIDA
APPLICATION FOR SITE PLAN REVIEW

DATE: April 25, 1997 NEW REVISED

NAME OF DEVELOPMENT HFI Developments, Inc.

APPLICANT	ENGINEER OR SURVEYOR	ARCHITECT
Name <u>HFI Developments, Inc.</u>	Name <u>Landry & Esber</u>	Name <u>Skirball Group, Inc.</u>
Address <u>6350 Gulf of Mexico Dr.</u> <u>Longboat Key, FL 34228</u>	Address <u>5940 Palmer Blvd.</u> <u>Sarasota, FL 34232</u>	Address <u>1509 S. Orange</u> <u>Ave.</u> <u>Sarasota, FL 34239</u>
Phone <u>(941) 383-5543</u>	<u>(941) 379-8831</u>	<u>(941) 951-1777</u>
Owner <u>Samual J. and Hilda G. Hood</u>		Phone <u>(519) 894-3222</u>

Address 162 Concession Street, Cambridge, Ontario, Canada N1R 2H7

Site Location 6350 Gulf Of Mexico Drive, Longboat Key, FL (Manatee County)

Zoning District C-1 Area of Site 1.30 Acres
% Ground Coverage 16.49% Total Units 2 Density 1.55 D.U./Acre

Proposed For Each Existing and New Building

BLDG A USE <u>Office + Apts.</u>	SQ.FT. <u>3,560</u>	HGT. <u>22.2 Ft.</u>	# FLRS <u>2</u>	1ST FLR ELEV. <u>5.0 Exist.</u>
BLDG B USE <u>Bank Retail Office</u>	SQ.FT. <u>5,168</u>	HGT. <u>24.0 Ft.</u>	# FLRS <u>1</u>	1ST FLR ELEV. <u>5.0* Addition</u>
BLDG C USE _____	SQ.FT. _____	HGT. _____	# FLRS _____	1ST FLR ELEV. _____
BLDG D USE _____	SQ.FT. _____	HGT. _____	# FLRS _____	1ST FLR ELEV. _____

PARKING REQUIRED 32 PARKING INDICATED 32

Attached hereto are the necessary approvals of the Federal, State, County and Regional Agencies. YES NO Is Public Hearing Required? YES NO
If Yes, hearing required, Comment: Site Plan Amendment Approval

Proposed complies with Comprehensive Plan and Zoning Ordinance. YES NO

Proposed complies with Subdivision Ordinance and Town Code. YES NO

Amount of Performance Bond Required _____

Amount of Maintenance Bond Required _____

I hereby certify that I have read and examined this application and know the same to be true and correct. All provisions of laws and ordinances governing this type of development will be complied with whether specified herein or not, the granting of a permit does not presume to give authority to violate or cancel the provisions of any other state or local law regulating construction or the performance of construction.

Applicant's Signature: *Jim Layfield (owner)*

Application Fee \$ _____ Receipt # _____ Date _____

Acknowledgement - Initial
Building Department _____ Planning & Zoning Board _____ Date _____
Commissioners _____ Date _____

Application and Plans Accepted By:

DATE: _____
Planning Official Signature _____

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

ICARD, MERRILL, CULLIS, TIMM,
FUREN & GINSBURG, P.A.

ATTORNEYS AND COUNSELLORS
2033 MAIN STREET, SUITE 600
SARASOTA, FLORIDA 34237
FACSIMILE (941) 366-6384
TELEPHONE (941) 366-8100

TAMPA TELEPHONE
(813) 221-2100
REPLY TO:
P.O. BOX 4195
SARASOTA, FLORIDA 34230

MICHAEL J. FUREN

April 18, 1997

Mr. Scott Pickett, Acting Planning & Zoning Official
Town of Longboat Key, Florida
610 General Harris Drive
Longboat Key, FL 34228

Re: Application to Amend the Site Plan Approval Resolutions
(Resolution 85-7 and Resolution 95-21) for Construction
of an Addition to the Existing Wedebrook Realty Company
Building at 6350 Gulf of Mexico Drive, Longboat Key,
Florida 34228.

Dear Scott:

Pursuant to and in accordance with the requirements of the
Longboat Key Zoning Code, this will verify and certify that as of
this date the record owner of the fee simple title to the real
property that is the subject of the above-referenced application
was as set forth in Exhibit "A" annexed hereto and incorporated
herein by reference. Exhibit "A" also sets forth each and every
other person having a legal ownership interest in the real property
that is the subject of the application.

Sincerely,

ICARD, MERRILL, CULLIS, TIMM,
FUREN & GINSBURG, P.A.


Michael J. Furen

MJF:jwc
Enclosure
cc: Mr. Jim Layfield
Mr. Mike Nink
f:\users\janac\wedebrook\pickett.own

EXHIBIT A

Owner: Samuel J. Hood and Hilda G. Hood, husband and wife.

Lessee: HFI Developments, Inc., a Florida corporation pursuant to Memorandum and Short Form of Long-Term Lease dated and effective as of September 29, 1994 and recorded in Official Records Book 1445, Page 680, Public Records of Manatee County, Florida.

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

April 11, 1997

Mr. Randy Fowler, Acting Building Official
Town of Longboat Key, Florida
610 General Harris Street
Longboat Key, FL 34228

Re: Application for Site Plan Amendment
Project: Wedebrook Realty Addition
6350 Gulf of Mexico Drive
Longboat Key, Florida 34228

Dear Mr. Fowler:

The undersigned are the owners of the real property included within the above-referenced Application for Site Plan Amendment.

Please be advised that HFI Developments, Inc., a Florida corporation, and its authorized agents, designees and consultants have been and are hereby authorized to file and process the above-referenced Application for Site Plan Amendment pursuant to the Town of Longboat Key's Zoning Code.

Thank you for your attention to this matter.

Sincerely,


Hilda G. Hood


Samuel J. Hood

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

EXHIBIT A-4

SITE PLAN CHECKLIST

EXHIBIT A-4

**HFI DEVELOPMENTS, INC.
 SITE PLAN CHECKLIST
 SECTION 158.097
 "APPLICATION FOR SITE PLAN APPROVAL"**

- (A) The character of the site will be an existing office building with two (2) accessory use dwelling units (apartments) on the second floor (3,560 SF) and a proposed addition with 5,532.6 SF. The proposed addition will have a 12' wide, asphalt drive-thru lane adjacent to the north side of the addition. The drive-thru lane will exit onto Wake Island road. The section of Wake Island Road from this exit point to Gulf of Mexico Drive will be rebuilt with an asphalt travel surface.
- (B) The site plan amendment for the project has been submitted as Sheet D-4 of Exhibit B-1 (attached). The site plan labels the title of the project as HFI Developments, Inc. The existing conditions plan (D-2), has contours at one-half foot intervals referenced to mean sea level. The following chart indicates additional information found on the site plan:

	<u>Proposed</u>	<u>Allowed</u>
Impervious Area (S.F.):		
Building Coverage	9,339 SF	
Existing Parking	5,850 SF	
Proposed Parking	4,821 SF	
Driveway, Drive-thru and Front Parking	<u>10,911 SF</u>	
	30,921 SF (54.6%)	45,302 (Max.) (80%)
Open Space (S.F.):		
Total Site	56,628 SF	
Total Impervious	(-) <u>30,921 SF</u>	
	25,707 SF (45.4%)	11,326 (Min.) (20%)
Front Setback (Ft.) (Existing Building)	41.6' (Min.)	45 (Min.)
Front Setback (Ft.) (Building Addition)	45.0' (Min.)	45 (Min.)
Rear Setback (Ft.)	281.6'	20 (Min.)
Side Setback (Ft.) (Existing Building)	25.8' (Min.)	25 (Min.)
Side Setback (Ft.) (Building Addition)	15.0' (Min.)	0 (Min.)
Building Separation *	N/A	30 (Min.)
MHWL Setback (Ft.)	N/A	20 (Min.)

* Proposed Building Addition.

- (C) An affidavit of ownership is included.

(D) We have shown on Sheet D-2 and D-4, Exhibit B-1 (attached), the relationship of the project to existing development in the area including streets, utilities, residential and commercial development, ecological features and important physical features in and adjoining this project.

(E) The proposed land use for the site is an existing office building with two (2) accessory use dwelling units (apartments) on the second floor and a proposed one story addition.

Existing Building	1st Floor	1,800 SF (Office)
	2nd Floor	1,980 SF (2 Apartments)
Proposed Building	1 Floor	1,100 SF (Retail) 3,818 SF (Office)

(F) We have provided the following tabulation of upland and wetland acreage and percentages:

	<u>Acres</u>	<u>Percent</u>
(1.) Jurisdictional, Landward of MHWL:	0.21	16.1
Uplands:	<u>1.02</u>	<u>83.9</u>
Total Project:	1.30	100.0
(2.) Total Site:	1.3 Acres	(56,628 SF)
Impervious:	<u>.71 Acres</u>	<u>(30,921 SF)</u>
Open Space:	.59 Acres	(25,707 SF)
Total Open Space % = 25,707 / 56,628 = 45.4%		
Provided 45.4%, Minimum Required 20%		
(3.) Total Site:	1.3 Acres	(56,628 SF)
Jurisdictional Lands:	.21 Acres (-)	(9,148 SF)
Impervious:	.71 Acres (-)	(30,921 SF)
Upland Open Space:	=	16,559 SF
Total Upland Open Space % = 16,559 / 56,628 = 29.2%		
Provided 29.2%, Minimum Required 15%		

(G) We have provided the architectural definition of the building envelope for the accessory use structures in Exhibit E-1 (attached).

(H) The type and location of all trees protected by Town regulation have been shown on Sheet D-6 of Exhibit B-1 (attached). All trees in the proposed parking area have been removed under Permit TP.00207 issued 7/28/95.

(I) We have shown the location of all the existing utilities on Sheet D-4 of Exhibit B-1 (attached).

(J) The landscape features of the project have been shown on the Sheet D-4 as part of Exhibit B-1 (attached). The existing landscape area per SPE 94-64 is noted on Sheet D-4 with the types and sizes of plantings. These plantings are along the south property line to the east end of the existing parking area, and along the east side of the existing parking area. Existing landscaping was also installed in the existing parking area island and the north side of the existing parking area. A row of 8-10 ft. oak trees were installed along the north property line and a hedge and landscaped area along the west property line was installed for the future (now proposed) parking area.

(K) We have shown the location of all pedestrian walks on Sheet D-3 of Exhibit B-1 (attached). Walkways are along the south side of the existing building, in the concourse and along the east and west sides of the building.

(L) We have included the location of all facilities on Sheet D-4 of Exhibit B-1 (attached). The disposition of all open space has been shown on this drawing. We have also included calculations indicating how the Town's open space requirements are being met. No recreation space is required. A list of these calculations is provided as follows:

Open Space:	0.59 Acres
Total Project Site:	1.30 Acres
% Open Space =	45.4%

(M) The waste disposal area is existing and shown on Sheet D-4 of Exhibit B-1 (attached), all other storage areas are totally enclosed. Asphalt with 6' opaque fence.

(N) A traffic analysis is not required. We have provided the location and dimension of the existing driveways on Gulf of Mexico Drive, on Sheet D-4 of Exhibit B-1 (attached).

Off Street Parking:

Existing Office	1,800 SF @ 1/250 SF = 7.2	Residential
Proposed Retail	1,100 SF @ 1/200 SF = 5.5	2-Apartments
Proposed Office	<u>3,818 SF @ 1/250 SF = 15.27</u>	
	6,718 SF 27.97	<hr/>
		2 Apartments
		2 (2 EACH) = 4

Parking Required:

$27.97 + 4 = 31.97 = 32$ Spaces

Parking Spaces Provided = 32 (Including 2 handicap)

Loading Area = 1

(O) The flood zone for the existing office building with two (2) accessory use dwelling units (apartments) and one proposed addition, that is being used is A-13, El. 10.0' as shown on FIRM Map Panel 125126-0005D (May 18, 1992). A surface water management plan based on best management practices has been provided on Sheet D-4 of Exhibit B-1 (attached). The surface water management plan consists of swales and a retention pond which provide for water quality, attenuation requirements is provided in the adjacent wetlands. The finished floor elevation will be 5.0' and the building addition will be flood proofed to elevation 10.0'.

(P) A soil erosion and sedimentation plan has been provided on Sheet D-4 of Exhibit B-1 (attached). Silt barriers during construction are shown.

(Q) A copy of the SWFWMD Permit 4614149.00 is on file, water and sewer service is existing. FDOT Permit 96-A-194-0017 is on file at the Town.

<u>Exhibit</u>	<u>Permit</u>	<u>Agency</u>
B-2	Wastewater Potable Water	FDEP (Not Required) FDEP (Not Required)
B-3	Storm Water Modification	SWFWMD
B-4	Driveway Connection for Wake Island Road	FDOT

Water and sewer services only are required; no permit is required for services.

FDOT Connection Permit 96-A-194-0017 has been issued for the Wedebrook Realty entrance.

(R) There are no additional data, maps, plans or statements necessary for the particular use and activity involved.

(S) There is no additional data pertinent to the site development plan.

(T) We are unaware of any additional material or information that the Town may require.

(U) Departures: (none required)

(V) This project is not a PUD.

EXHIBIT A-4

**HFI DEVELOPMENTS, INC.
PERFORMANCE STANDARDS FOR SITE & DEVELOPMENT PLANS
SECTION 158.102
"APPLICATION FOR SITE PLAN APPROVAL"**

- (A) The character of the site will be an existing office building with two (2) accessory use dwelling units (apartments) on the second floor (3,560 SF) and a proposed addition with 5,532.6 SF. The proposed addition will have a 12' wide, asphalt drive-thru lane adjacent to the north side of the addition. The drive-thru lane will exit onto Wake Island road. The section of Wake Island Road from this exit point to Gulf of Mexico Drive will be rebuilt with an asphalt travel surface.
- (B) The site and structures are designed to be coordinated to be pleasing to the eye and harmonious to the overall effects of the area. The Architectural Plans are included as Exhibit C-1, attached.
- (C) The calculations for floor area ratio are not required for commercial buildings.
- (D) **Parking, Internal Circulation and Access to Public Roads.**
The parking requirements for the site area are shown on Plan Sheets C-4 and C-5 and calculated herein to be thirty-two parking spaces. The spaces are in one existing parking bay and one proposed parking bay at the rear of the building and four separate parking spaces in front of the building. The internal traffic circulation for the parking bays is evident from the plan; the circulation to the drive-thru lane is through the existing parking bay and turning to the west around a 24' radius curve onto the drive-thru lane. The 24' radius is consistent with the turning radius into a parking space with a normal speed of less than 15 MPH. This again is consistent with transportation engineering requirements. Once passing through the drive-thru lane, the car will pass a stop sign, stop bar and a sign to designate 2-way traffic ahead prior to pulling out onto Wake Island Road, an alley. The alley has a 20' right-of-way and a new asphalt travel area will be constructed to Gulf of Mexico Drive. The new travel area will be a 20' inverted crown roadway. The suggested speed limit on the roadway should be 15 MPH. The only traffic on Wake Island Road will be from the HFI Developments building (Wedebrock Realty) and the Town of Longboat Key Public Works and Planning, Zoning and Building complex. Wake Island Road (alley) is a public platted way leading directly to Gulf of Mexico Drive, a major public thoroughfare. The pedestrian traffic will be from the existing building to the south along an existing sidewalk to the parking area or from the concourse to the front or rear parking areas. There will be no access to the drive-thru lane directly from the building.

The following is a discussion of the safety aspects of the drive-thru and Wake Island Road (alley). This facility complies with Code Section 158.131 "Drive In Facilities". The exit is in excess of 70' to an intersection (Gulf of Mexico Drive). The egress from the drive-thru

lane does not extend across a sidewalk or curb. The location of the drive is consistent with the code. It is estimated that 95% of the traffic on Wake Island Road (alley) will be during normal working hours which are during daylight hours. There are pole mounted lights at the Town of Longboat Key complex which will light the east section of Wake Island Road (alley).

The surface of Wake Island Road (alley) is presently shell and extends outside the 20' right-of-way to the north. We anticipate that by improving the westerly section of the alley way, a certain amount of liability for the Town would be reduced if the traffic did remain within the R/W. The alley way from the drive-thru exit to Gulf of Mexico Drive will be rebuilt with an asphalt surface course. This will also be a benefit to the traffic from the Town's complex on General Harris Street which use Wake Island Road (alley) for access and parking at the complex.

There will be a concrete barrier curb along and outside the R/W which will give a positive delineation between the drive-thru lane and Wake Island Road (alley). Attached are articles from three (3) transportation engineering books indicating that curbs are a driving barrier and a separation device between driving lanes. A recommended Wake Island Road speed of 15 MPH will also benefit the safety aspect.

At the point of egress from the drive-thru lane, there will be a stop sign and stop bar and a sign indicating 2-way traffic ahead on Wake Island Road (alley). On Wake Island Road (alley) at Gulf of Mexico Drive, there will be a stop sign, stop bar, sidewalk crossing striping and a street sign.

As noted previously, Wake Island Road (alley) is the prime ingress and egress for the vehicles from Public Works.

- (E) **Traffic Impacts**
Traffic impacts to the site have been discussed above and are in compliance with the code.
- (F) **Open Space and Landscape**
A Landscape Plan, Sheet D-7 is included in Exhibit B-1 and shows that 29% of the site is landscape while non-residential sites require 20%. The open space calculated in Exhibit A-4 is 45.4% while the minimum required is 20%.
- (G) **Wetland Development Restrictions**
A jurisdictional survey was conducted by SWFWMD and the Army Corps of Engineers. The designated area is shown on Plan Sheet D-4 and no development in that area will take place.
- (H) **Surface Water Management**
An existing SWFWMD permit exists and a modification to that permit has been requested for the additional impervious area of the drive-thru lane and Wake Island Road (alley).

- (I/J) Potable Water and Wastewater
Water and wastewater service area available on the site.
- (K) Soil erosion and sedimentation control Plan Sheet D-4 shows this erosion and sedimentation plan. Silt barriers will be installed around the construction site prior to commencement.
- (L) Supplemental Controls for Multi-Family, Residential or Tourism Uses
This section is not applicable.

GREEN BOOK (FDOT)
SECTION III - GEOMETRIC DESIGN

The general policy to be followed is that guardrails should be used if impact with the guardrail is less likely or less severe than impact with roadside objects. Further requirements and design criteria for guardrails are given in Section IV ROADSIDE DESIGN.

f. Curbs

Curbs may be used to provide drainage control and to improve delineation of the roadway. They should not be used for delineation or traffic control on high speed facilities.

Where curbs are used on facilities with speeds greater than 40 mph, they should be of the mountable rather than the barrier type. The height of mountable curbs shall not exceed 6 inches. Further design criteria and requirements for curbs are given in Section IV ROADSIDE DESIGN.

g. Parking

Parking on street rights of way should be avoided.

h. Right-of-Way

The acquisition of sufficient right-of-way is necessary in order to provide space for a safe street or highway. The width of the right-of-way required depends upon the design of the roadway, the arrangement of bridges, underpasses and other structures, and the need for cuts or fills. The right-of-way acquired should be sufficient to:

a. Allow development of the full cross section including adequate medians and roadside recovery areas.

b. Provide for the layout of safe intersections, interchanges and other access points.

c. Allow for adequate sight distance at all points, particularly on horizontal curves at an intersection and other access points.

d. Provide, where appropriate additional buffer zones to improve roadside safety, noise attenuation and the overall esthetics of the street or highway.

e. Provide adequate space for placement of necessary pedestrian and bicycle facilities.

f. Allow for future lane additions, increases in cross section or other improvements. Frontage roads should also be considered in the ultimate development of many high volume facilities.

g. Provide for construction of future grade separations or other intersection improvements at selected crossroads.

TRAFFIC ENGINEERING By LOUIS J. PIGNATARO

Highway Cross-Section Elements

Lane Widths

These have a great influence on the safety and comfort of driving. Ten- to 12-foot lane widths are standard, and the tendency is to use the larger value with the continued upward trend in traffic volumes, vehicle speed, and widths of trucks. Lane widths narrower than 12 feet can adversely affect capacity and safety, so their use should be limited to other than high-speed, high-volume facilities.

Lane widths of 13 and 14 feet have been used on some high-speed, rural, two-lane roads. Attempts to use wider lanes lead to the practice of some drivers of using such roads as multi-lane facilities.

Normal Cross Slopes

These have a crown or high point in the middle and slope downward toward both edges to facilitate drainage. The downward cross slope may be a plane or curved section, or a combination of curve and plane. Curved cross slopes usually are parabolic. On divided highways each one-way pavement may be crowned separately, as on two-lane highways.

Since many highways are on a tangent or a flat-curve alignment, the rate of cross slope is an important element in cross-section design. Rates should be as low as practicable for vehicle operation, but they must be high enough for proper drainage. Where two or more lanes are inclined in the same direction on multi-lane pavements, each successive lane outward from the crown line should preferably have an increased slope. The lane adjacent to the crown line should be pitched at the normal minimum slope and on each successive lane outward the rate should be increased by $\frac{1}{8}$ inch per foot. Normal cross slopes vary from a minimum of $\frac{1}{8}$ inch per foot on high-type pavement surfaces to $\frac{1}{4}$ inch per foot.

Shoulders

Adjacent to the traveled way shoulders are provided for the accommodation of stopped vehicles, for emergency use, and for lateral support of the base and surface courses. The need for shoulders is determined by the type of highway, the traffic volume, the speed of traffic, traffic composition, and the type of terrain. Shoulders vary from a minimum width of 4 feet to a desirable width of 12 feet for heavily traveled and high-speed roads. Full-width usable shoulders (8 to 12 feet) should be provided on highways where the DHV exceeds 100 vehicles per hour.

An important element of shoulder design is its cross section. Normally shoulders are sloped to drain away from the traveled way, and are an important link in the lateral surface runoff system. Shoulders must be sloped sufficiently to remove surface water from the pavement areas, but not to the extent that vehicle use would be hazardous.

Sidewalks

In the city, sidewalks are accepted as integral parts of the streets. However, the need for sidewalks is great in many rural areas because high speeds and a general lack of adequate lighting make it hazardous to walk on the traveled way. The most pressing need for sidewalks in rural and suburban areas is at points of community development, such as schools, local businesses, industrial plants, etc. Justification for sidewalks in these areas is dependent upon the vehicle-pedestrian hazard, which is governed chiefly by the volume of pedestrian and vehicular traffic and the speed of vehicular traffic. In general, wherever the roadside and land development conditions are such that pedestrians regularly move along a main or high-speed highway, sidewalks should be provided.

Curbs

By their design and location curbs can appreciably affect drivers, and therefore the safety and utility of a highway. Curbs are used to control drainage, prevent vehicles from leaving the pavement at hazardous points, delineate the edge of the pavement, present a more finished appearance, and assist in the more orderly development of the roadside.

Curbs are needed most on highways in urban areas. In strictly rural areas there is extensive mileage without curbs, but curbs are frequently used in rural design for one or more of the purposes described.

There are two general classes of curbs: barrier and mountable. Barrier curbs are relatively high and steep-faced, designed to prevent vehicles from leaving the pavement. They range from 6 to 20 inches in height, and may have a two-step section. Barrier curbs are used on bridges, piers, sidewalks (maximum height of 8 inches to prevent scraping of bumpers), and in some instances on medians. Mountable curbs are low curbs or curbs with flat, sloping faces, or both, and are designed so that vehicles can cross them without a severe jolt. Mountable curbs are used primarily on medians, at the inside edge of shoulders, and to outline channelizing islands in intersection areas.

HIGHWAY ENGINEERING
B. CLARKSON H. OGLESBY CHANNELIZATION 303

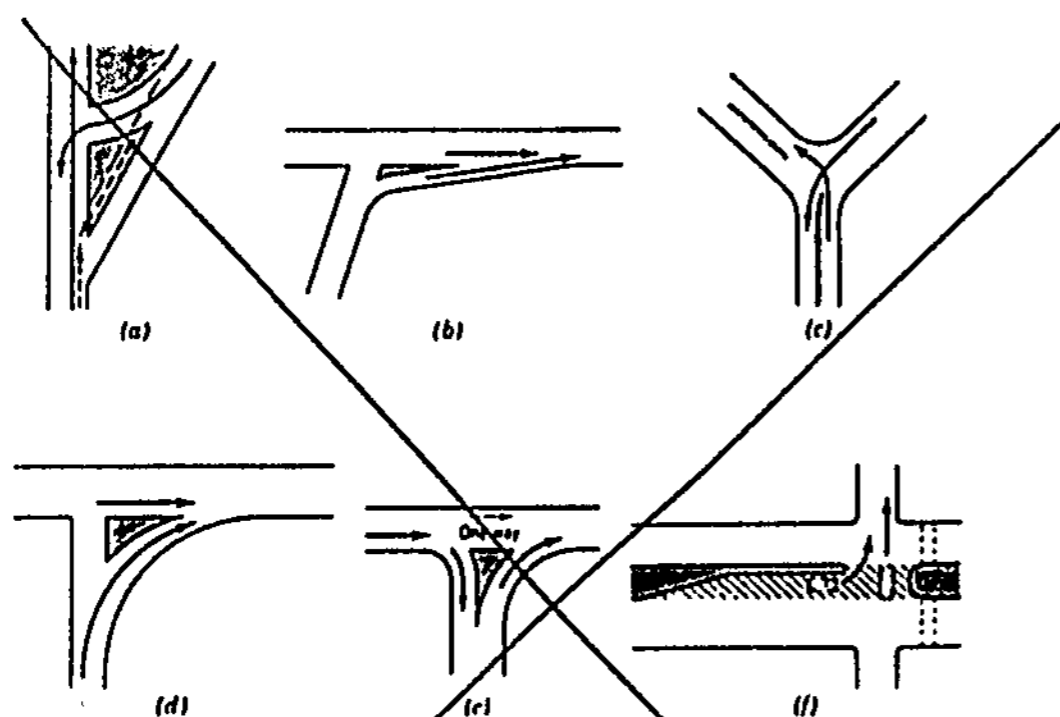


FIGURE 9-12. Channelization techniques.

5. By channelization, prohibited turns may be prevented (Fig. 9-12(e)).
6. By channelization, refuge may be provided for turning or crossing vehicles and for pedestrians. This is illustrated by the provision of turning lanes and of protected areas for cross traffic and pedestrians at the center of the street (see Fig. 9-12(f)).
7. By channelization, points of conflict may be separated in such a way that the driver faces only one decision at a time. This reduces confusion and accidents because the driver can reach the proper decision in a shorter period of time.
8. Channelizing devices provide protected locations for essential traffic-control devices, such as signs and signals.

Channelizing Devices

The various channelizing devices exert different degrees of control over driver and vehicle. On the one hand, pavement markings and changes in roughness or color of the pavement surface merely suggest the appropriate path and speed.²² On the other hand, nonmountable curbs, guard rail, fences, and bumper blocks positively prohibit encroachment by the vehicle under usual circumstances. Between the extremes fall such devices as raised bars made of concrete or bituminous mixtures, or mountable curbs over which a vehicle may pass at low speed if the driver so desires. In selecting the device that will work best in a given situation, the designer must weigh such factors as the space available for the installation, its cost, traffic volumes and speeds, the seriousness of the accidents that may result from vehicles entering the prohibited area, and the influence of police supervision on

²²See Chap. 10 for further discussion of pavement markings.

driver behavior. For example, all these conditions differ greatly between rural and urban locations, and designs appropriate for one may not fit the other.

Curbing and raised islands bounded by curbs are particularly important channelizing devices. Figure 9-13 shows typical curb and median barrier cross sections.²⁴ The given dimensions are intended merely as guides, as curb details are not standardized among highway agencies. In setting curb heights it is important to note that skirt and fender edges or other low-hanging parts of passenger automobiles will catch curbs higher than 6-7 in.

Curbs of the configurations shown in Fig. 9-13 are commonly constructed of concrete, although asphalt concrete or granite or other quarried stone are sometimes used. Almost all configurations are sometimes constructed as single curbs, meaning that the curb is constructed first and the pavement is placed against it (see Fig. 9-13(a)). Generally, where water is to flow along the gutter, combined curb and gutter (see Fig. 13(d)) is to be preferred. Curb and gutter being placed

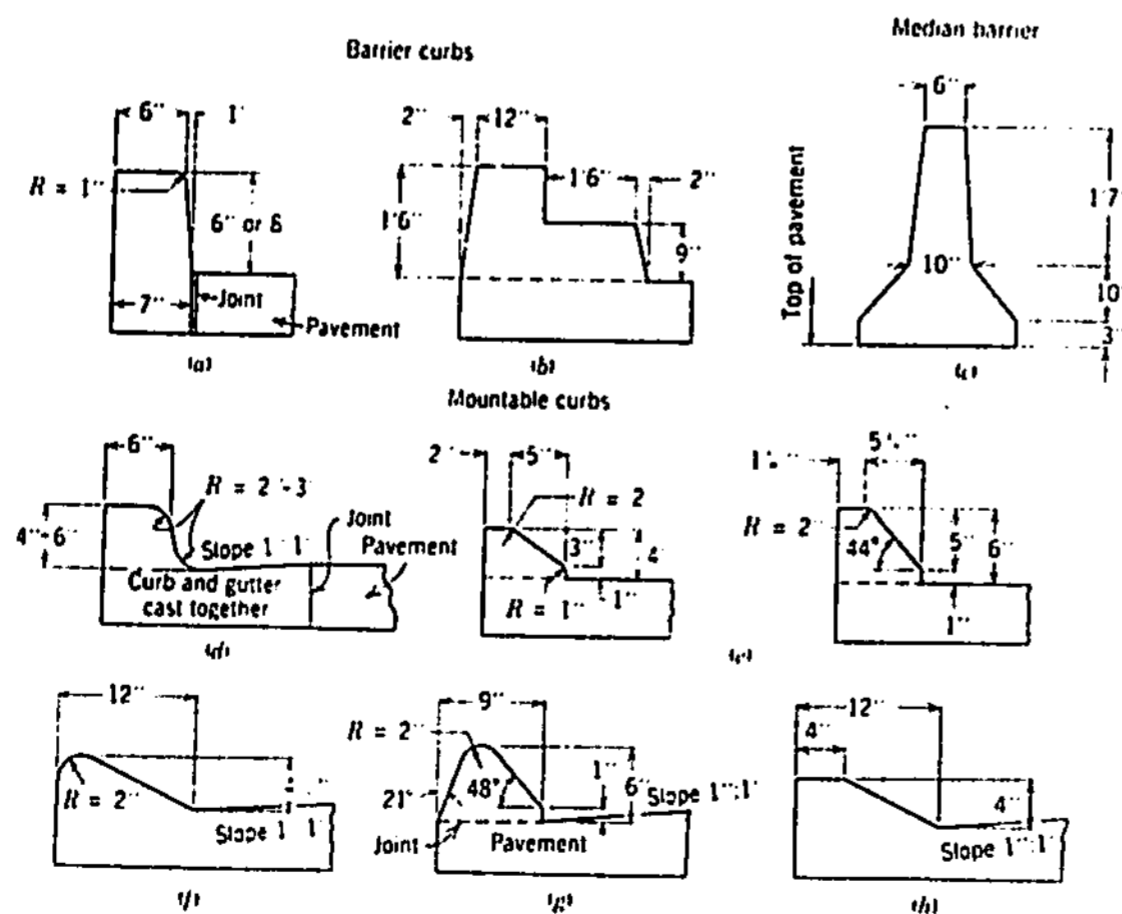


FIGURE 9-13. Typical highway curbs.

²⁴ The standard drawings of each highway agency include details of many curbing types, each of which is assigned a code number or letter. The roadway designer selects the type that meets the particular situation and designates it on the plans by giving its number. E. C. Lokken, *Transportation Engineering Journal*, ASCE, Feb. 1974, pp. 151-168, treats concrete median barriers in detail.

DESIGN OF INTERSECTIONS AT GRADE 305

first as a separate operation provides an excellent means for establishing proper pavement grades.

Special machines have been developed that form the curb by extruding concrete or asphaltic materials as the machine passes by. These are particularly effective where the curbing is to be placed on existing paving. In other instances precast curbing sections, stuck to the paving with epoxy resins, have been used to advantage.

In many situations, high visibility is an important requirement for curbings. In these cases it is common practice to use white Portland cement in the concrete or to paint the curbing. Many agencies further increase visibility by providing indentations in the curb face which will reflect headlight rays back to the driver. Installing special reflectors in the curb face offers another method of increasing visibility.

Guard rail provides a positive means of confining vehicles to designated roadways and on occasion are used as a channelizing device. But vehicles striking them will suffer some damage so that installations, if made, should be some distance removed from the traveled way.

DESIGN OF INTERSECTIONS AT GRADE

Except for freeways, all highways have intersections at grade, so that the intersection area is a part of every connecting road or street. In this area must occur all crossing and turning movements. Figure 9-14 schematically portrays typical intersections at grade, ranging from simple to complex. The unchannelized intersections, shown down the left side of the figure, are cheapest and least elaborate. With them, the intersecting roadways have been joined by circular arcs in order to provide pavement under vehicles turning to the right. For intersections of roads or streets carrying little traffic, no further treatment is needed, with the possible exception of curbings to keep vehicles on the pavement and to channel surface drainage and signs or other traffic control devices. Flared designs involve (a) widening the entering traffic lanes to permit deceleration clear of through traffic and (b) widening the exit lanes to provide for acceleration and merging. Channelized designs are intended to direct approaching drivers to the correct paths by employing the principles of channelization enumerated above. Some of the arrangements shown in Fig. 9-14 could create head-on conflicts between opposing movements. For this reason they are appropriate only under traffic-signal control.

Figure 9-15 shows several schematic layouts for the intersection of a cross street with a divided expressway with frontage roads. It can be seen that acceleration and deceleration lanes to clear the through lanes of both right-turn and left-turn vehicles, channelizing islands, and pavement markings have been used.

A careful traffic count and estimate for the future, including data regarding each turning movement, must precede the design of an important intersection. Only

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

EXHIBIT A-5

LETTER TO
TOWN OF LONGBOAT KEY

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

LANDRY & ESBER

Engineers • Surveyors

April 22, 1997

Mr. Griff Roberts, Town Manager
Town of Longboat Key
501 Bay Isles Road
Longboat Key, FL 34228

Re: H.F.I. Developments, Inc.

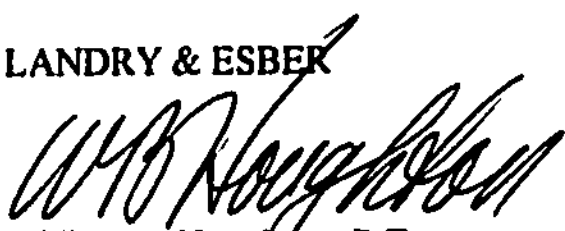
Dear Mr. Roberts:

For the referenced project, we request in accordance with Section 158.099(A)(2), "Submission Procedures" of the Longboat Key Zoning Code, that the Town waive the requirement for obtaining all County, Regional, State, and Federal Permits prior to submission of the application for site plan amendment review, thereby allowing concurrent approval of the various permit applications. A copy of the existing SWFWMD permit modification application for stormwater and FDOT driveway connection application are included in our site plan application package. DEP permits for potable water and sanitary sewer are not required as they are existing.

We would appreciate your review and approval of this request at your earliest convenience. If you should have any questions concerning this request, please do not hesitate to contact us.

Sincerely,

LANDRY & ESBER


William B. Houghton, P.E.
Vice President

Enclosures

C:\WPWIN60\WPDOCS\B01-8025\B02502\EXHIB-A5.WPD April 15, 1997 (B02502)

Fax (941)379-0556 • 5940 Palmer Blvd. • Sarasota, FL 34232 • (941)379-8831

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

EXHIBIT A-6

DRAINAGE ANALYSIS

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

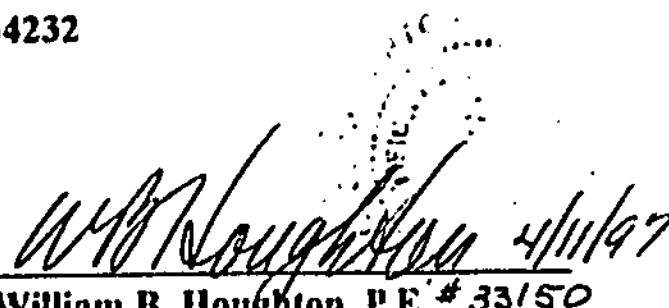
**DRAINAGE REPORT
FOR
HFI DEVELOPMENTS, INC.
GULF OF MEXICO DRIVE
LONGBOAT KEY, FLORIDA**

PREPARED FOR:

**H.F.I. DEVELOPMENTS, INC.
6350 GULF OF MEXICO DRIVE
LONGBOAT KEY, FL 34228**

PREPARED BY

**LANDRY & ESBER
5940 PALMER BLVD.
SARASOTA, FLORIDA 34232**


William B. Houghton, P.E. # 33150

**MARCH, 1995
REVISED APRIL 1, 1997**

I. PRE-DEVELOPMENT

A. DESCRIPTION OF CONDITIONS

The project site is a portion of Lots 20A and 21A, Subdivision of Longboat Key, per Plat Book 7, Page 16, Public Records of Manatee County, Florida, lying easterly of Gulf Of Mexico Drive in Section 23, Township 35 South, Range 16 East.

The parcel is approximately 490 feet long and 115 feet wide, consisting of 1.30 acres more or less of which 0.24 acres are jurisdictional and not included in these calculations.

There is an existing building and parking lot on the westerly end of the parcel, 1 parking bay to the east and the remainder is vacant. Jurisdictional land lies to the east end of the property.

The ground cover consists of trees and native low shrubs and grass.

A-1. (REVISION)

The permitted parcel contains 1.30 Acres. A portion of the addition building has been leased to a bank which will include a drive-thru window on the permitted parcel. The Town of Longboat Key is requiring that a section of the alley to the north be upgraded and the runoff from this section be treated on the site. The added area is 2,860 SF (0.066 Ac.) which totals 1.366 Acres.

II. POST-DEVELOPMENT

A. DESCRIPTION OF CONDITIONS

The proposed project consists of constructing an addition to the existing building and adding one parking Bay.

The stormwater treatment will be a dry, percolating system for treatment only. There will be two areas with the roof drains into the front treatment area and the rear will be impervious runoff treatment. The system is designed to treat the first one inch of rain plus a factor of 50% for the entire site. No attenuation is required as the receiving water is Sarasota Bay.

Total Site 1.30 Ac
Jurisdictional - .24
Total Stormwater 1.06 Ac

1.06 Ac @ 1" of rainfall + 50% = 5,772 CF

A-1. (REVISION)

Total Site 1.366 Ac
Jurisdictional - .24
Total Stormwater 1.126 Ac

1.126 Ac @ 1" of rainfall + 50% = 6,131 CF
Increased Treatment of 359 CF

STORMWATER DESIGN DATA SUMMARY B.

PROJECT: HFI DEVELOPMENTS, INC.

BASIN NO. - POND NO.	# 1	# 2	#3
POND BOTTOM ELEVATION	3.2	2.5	3.0
SEASONAL HIGH WATER ELEVATION			
CONTROL DEVICE ELEVATION			
DESIGN LOW WATER ELEVATION	DRY	DRY	DRY
WEIR INVERT ELEVATION			
DESIGN HIGH WATER ELEVATION	4.4	3.45	3.4
TOP OF BANK ELEVATION	4.45	3.5	3.5
AREA @ TOP OF BANK (SF)	3514	3317	468
VOLUME @ DHW (CF)	3340	2656	135
VOLUME @ TOB (CF)	3475	2689	187
TREATMENT AREA (AC)			
TREATMENT VOL. REQUIRED (CF)	3340	2656	135
TREATMENT VOL. PROVIDED (CF)	3475	2689	187
METHOD OF TREATMENT	PERC.	PERC.	PERC.
CONTROL DEVICE TYPE			
CONTROL DEVICE DIMENSIONS			
RECOVERY TIME (HRS)	63.0	111.0	26.1
100 YEAR FLOODPLAIN	ENCROACHMENT (AC-FT)		
	COMPENSATION (AC-FT)		

NOTE: RECEIVING WATERS SARASOTA BAY
 NO ATTENUATION REQUIRED

	# 1	# 2	# 3
ARDAMAN	DR-1 (HA-1)	DR-2 (HA-2)	DR-1 (HA-1)
PERC. RATE	7.9 IN/HR	3.6 IN/HR	7.9 IN/HR
EPA PUBLICATION RATE	3%	3%	3%
ACTUAL PERC RATE	0.23 IN/HR	0.108 IN/HR	0.23 IN/HR
POND DEPTH	1.2 FT	1.0 FT	0.5 FT
RECOVERY TIME	63.0 HR	111.0 HR	26.1 HR

EXHIBIT A-7

TRAFFIC IMPACT ANALYSIS

EXHIBIT A-7

**HFI DEVELOPMENTS, INC.
POST, BUCKLEY, SHUH & JERNIGAN, INC.
SITE PLAN REVIEW DATED MAY 2, 1997**

1. The adequacy of the turning radius for northbound vehicles entering the Wedebrook Realty property and Wake Island Road (an alley) is set by the sharp angle between Gulf of Mexico Drive and the property lines and rights-of-way lines of the Wedebrook property and Wake Island Road (an alley). This acute angle makes it virtually impossible to design a turning radius within normal transportation engineering criteria. Improvements to the Wake Island Road (an alley) and Gulf of Mexico Drive intersection are restricted by extremely limited right-of-way. The entrance at the Wedebrook Realty site was approved previously by the Town Commission and was designed and approved as directed by the Town of Longboat Key in accordance with Town Code. The Applicant's proposal to improve a portion of Wake Island Road (an alley) will improve its existing intersection with Gulf of Mexico Drive.
2. The traffic generated from the existing Wedebrook building and the proposed addition have been approved previously by the Town Commission. The only issue the Post, Buckley, Shuh & Jernigan review should have addressed pertains to the proposed Amendment to the Site Plan to provide a drive-thru lane. All other traffic issues have been resolved previously. This Amendment is not requesting an increase in size for any building or addition. The quantity of pavement proposed for Wake Island Road (an alley) was designed in accordance with a previous agreement from Town Staff. The client will not agree to pave beyond the drive-thru exit.
3. The traffic flow arrows have been revised to more clearly designate the traffic conditions moving toward and through the bank drive-thru lane.
4. The stormwater flow pattern across the easterly parking bay north entrance has been revised.
5. The original request by Town Staff to our client was to provide adequate protection between vehicles on Wake Island Road (an alley) and the vehicles in the bank drive-thru lane. Our design and the supporting documents discuss the use of curbing to channel vehicles within the roadway, the original objective, and we feel this design is adequate. There also is a row of Oak Trees between the drive-thru lane and Wake Island Road (an alley) which further delineates the drive-thru and the alley. We must again note that the only traffic on Wake Island Road (an alley) is from the Town's complex on General Harris Street and the Wedebrook building and addition.

In our opinion, the turning radius into the drive-thru lane is no sharper than many street corners in Town and certainly less sharp than the turning radius into many parking lots. The turn into the drive-thru lane which should be negotiated at approximately 5 to 10 MPH is certainly equal to turning a vehicle into a parking space which is perpendicular to the traffic flow. The client will install a flexible plastic delineator on the parking stall side line nearest the bank drive-thru. The delineator will be approximately 4 feet tall to be visible from within a vehicle. The client will also install a short length of vertical curb on the outside edge of the drive-thru to help channel vehicles into the drive-thru, even though the delineator curb at the right-of-way line is a foot away. The client will not agree to redesign the approved parking lot.

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

EXHIBIT A-7
HFI DEVELOPMENTS, INC.
SITE PLAN CHECKLIST
TRAFFIC IMPACT ANALYSIS

A TRAFFIC IMPACT ANALYSIS IS NOT REQUIRED.

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

ICARD, MERRILL, CULLIS, TIMM,

FUREN & GINSBURG, P.A.

ATTORNEYS AND COUNSELLORS

2013 MAIN STREET SUITE 600
SARASOTA, FLORIDA 34237

PHONE 342-6300

FACSIMILE

PHONE 342-6300

REPLY TO: PO BOX 4105
SARASOTA, FLORIDA 34230

400 NORTH TAMPA SUITE 2000

TAMPA FLORIDA 33600

PHONE 229-2200

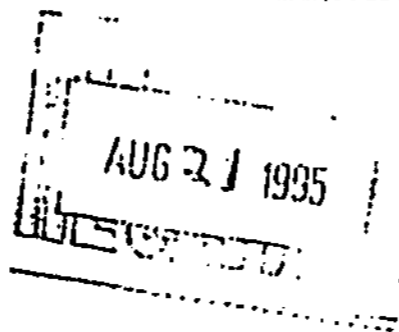
FACSIMILE

TAMPA FACSIMILE

PHONE 229-0550

MICHAEL J. FUREN

August 17, 1995



Mr. Daniel Gaffney
Planning, Zoning & Building Director
Town of Longboat Key, Florida
610 General Harris Drive
Longboat Key, FL 34228

Re: Application for: Site Plan Approval
Project: Building Addition at
Wedebrook Realty
Site Location: 6350 Gulf of Mexico Drive, Longboat
Key, Florida 34228
Applicant: HFI Developments, Inc., a Florida
corporation

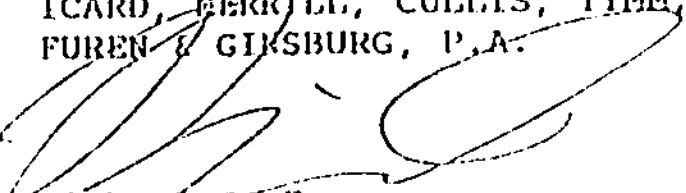
Dear Dan:

This will confirm that pursuant to Section 158.097 of the Longboat Key Zoning Code, you have determined that a traffic impact analysis shall not be required for this particular development as the Town has recently assessed all of the traffic impacts at buildout of the Town as part of the "EAR" process.

We appreciate not having to go through this expensive academic exercise when the Town already has an in-house analysis indicating there will be no need for additional improvements along Gulf of Mexico Drive to support this building addition.

Sincerely,

ICARD, MERRILL, CULLIS, TIMM,
FUREN & GINSBURG, P.A.


Michael J. Furen

MJF:jwc

cc: Mr. Jim Layfield
Mr. Bob Marischen
Mr. Sam Esber
Mr. Phil Skirball

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

EXHIBIT B-1

**SITE PLANS
(ATTACHED)**

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

EXHIBIT B-1 (ATTACHED)

AMENDED SITE DEVELOPMENT PLAN:

<u>DESCRIPTION</u>	<u>SHEET NUMBER</u>
Cover Sheet	D-1
Existing Conditions	D-2
Aerial Photo	D-3
Site Plan Amendment	D-4
Site Data	D-5
Tree Location	D-6
Landscape & Stabilization Plan	D-7

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

EXHIBIT B-2

**FDEP PERMIT FOR WASTEWATER
COLLECTION/TRANSMISSION SYSTEM
AND DRINKING WATER DISTRIBUTION
SYSTEM ARE NOT REQUIRED.**

**WATER & SEWER IS EXISTING AND
SERVICES ONLY DO NOT REQUIRE A PERMIT.**

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

EXHIBIT B-3

SWFWMD
CONSTRUCTION PERMIT MODIFICATION
APPLICATION

LANDRY & ESBER

Engineers • Surveyors

April 11, 1997

Ms. Mary French
Southwest Florida Water Management District
115 Corporation Way
Venice, Florida 34292-3524

Re: Wedebrook Realty - Manatee County;
Permit 4614149.00

Dear Mary:

Enclosed are four (4) each revised Site Plan and Drainage Report for the Wedebrook Realty project. On behalf of the owner, we are requesting a letter modification to revise the permit as follows:

- a) Installation of a drive-thru lane for proposed bank.
- b) Installation of approximately 140 LF of paved alley which will drain into the proposed retention area in front of the building.
- c) The original plan showed a shallow swale type retention area on the north side of the building. This area has been removed and the area in front of the building increased in volume to compensate. A small area at the rear of the proposed building has been added.

These revisions are part of the site plan approval by the Town of Longboat Key and the treatment for the alley within the site was at their request.

If you have any questions, please contact us at any time.

Yours truly,

LANDRY & ESBER



William B. Houghton, P.E.
Vice President

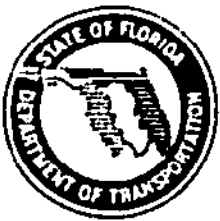
C:\WP\WING\WPDOCS\B01-0025\B02502\FRENCH.WPD

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

EXHIBIT B-4

**FDOT
DRIVEWAY CONNECTION PERMIT
APPLICATION**

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE



STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRIVEWAY/CONNECTION APPLICATION
FOR ALL CATEGORIES

FORM 850-040-15
SYSTEMS PLANNING - 11/94
Page 1 of 6

OFFICE USE ONLY

Application Number: _____	Accepted By: _____ <small>FDOT STAFF (TYPE OR PRINT)</small>
Category: _____	Date: _____

APPLICANT COMPLETE REMAINDER OF FORM

PART 1: APPLICANT INFORMATION (Please type or print)

APPLICANT: <u>Town of Longboat Key</u> Mailing Address: <u>501 Bay Isles Road</u> City, State, Zip: <u>Longboat Key, FL 34228</u> Telephone: <u>(941) 316-1999</u>	CONSULTING FIRM/PROJECT MANAGER: <u>William B. Houghton, P.E.</u> <u>Landry & Esber, Inc.</u> Mailing Address: <u>5940 Palmer Blvd.</u> City State, Zip: _____ <u>Sarasota, FL 34232</u> Telephone: <u>(941) 379-8831</u> FAX, Mobile Phone, etc.: <u>(941) 379-0556</u> <small>CIRCLE ONE</small>
PROPERTY OWNER: <u>Town of Longboat Key</u> Responsible Corporate Officer: <u>Mr. Griff Roberts</u> Mailing Address: <u>501 Bay Isles Road</u> City, State, Zip: <u>Longboat Key, FL 34228</u> Telephone: <u>(941) 316-1999</u> FAX, Mobile Phone, etc.: _____ <small>CIRCLE ONE</small>	

ARE YOU AN AUTHORIZED REPRESENTATIVE?

If the property owner desires to have a representative sign and handle the application, the owner must provide a notarized letter of authorization attached with the application. (SEE PART 2 OF THIS FORM BELOW)

PART 2: APPLICANTS AUTHORIZATION OF REPRESENTATIVE

Authorized Representative:

(NOTE: All correspondence will be made through the representative. A notarized letter of authorization must be attached if you use an authorized representative.)

Name: _____

Company: _____

Address: _____

Continue onto next page

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRIVEWAY/CONNECTION APPLICATION
FOR ALL CATEGORIES

FORM 850 0-13
SYSTEMS PLANNING - 11/74
Page 2 of 6

APPLICANT (Continued)

IF YOU EXPECT 600 TRIPS PER DAY OR LESS FROM YOUR SITE,
COMPLETE THIS SECTION BELOW:

PART 3: DESCRIPTION OF PROPOSED USE

CATEGORY A

- Single Family Home
 Duplex
 Other use less than 21 vehicles per day USE: _____

CATEGORY B

- Dwelling units (between 3 and 60)
 Office/Commercial/Institutional in a small structure (enter type and gross square feet below).

Not to exceed
600 trips/day

TYPE OF USE _____

SQUARE FEET OF STRUCTURE _____

Other

- Wake Island Road (20' alley)
Description of Use

IF YOU EXPECT MORE THAN 600 TRIPS PER DAY FROM YOUR SITE,
COMPLETE THIS SECTION BELOW:

Land Use	Units (Gross Sq. ft. or Dwelling Units)
Existing: _____	_____
Proposed: _____	_____

Attach additional sheets if necessary

PART 4: TRIP GENERATION INFORMATION

Estimated Average Daily Volume: _____ (Also used by Town of Longboat Key Public Works)
(Not required for Category A)

Estimated Average Peak Hour Volume: IN _____ OUT _____ (AM/PM) (Circle AM, PM, Other) _____

Continue onto next page

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRIVEWAY/CONNECTION APPLICATION
FOR ALL CATEGORIES

FORM 850-040-15
SYSTEMS PLANNING - 11/94
Page 3 of 6

APPLICANT (Continued)

IF YOU ARE APPLYING FOR A TEMPORARY, PUBLIC STREET, OR SAFETY
UPGRADE PERMIT COMPLETE THE FOLLOWING:

CHECK ONE

- Temporary Permit, Description: _____
From (Date): _____ To (Date): _____ (Not to exceed 6 months)
- Public Street, Road or Facility, Description: _____ Expected Daily Traffic _____
- Safety Upgrade, Description: _____

ATTACH EXTRA SHEETS IF NEEDED

PART 5: LOCATION INFORMATION

Property Physical Site Address: Wake Island Road (20' alley)

Between: General Harris Street and Edlee Lane
Street, Road, Etc. and Street, Road, Etc.

If development is in phases, please provide this information on a separate sheet.

Please indicate North

IF IT WILL SERVE TO BETTER COMMUNICATE,
PLEASE PROVIDE SKETCH OF PROPERTY LOCATION - (SCALE IS NOT IMPORTANT)

PART 6: HIGHWAY AND CONNECTION LOCATION INFORMATION

CONNECTION NO. 1	CONNECTION NO. 2
Road Name: <u>Gulf Of Mexico Drive</u>	Road Name: _____
Roadway Access Management Classification _____ <small>(Available at Department)</small>	Roadway Access Management Classification _____ <small>(Available at Department)</small>
Posted Speed Limit: <u>45</u> MPH	Posted Speed Limit: _____ MPH
Road Section Number: <u>13080</u> <small>(Available at Department)</small>	Road Section Number: _____ <small>(Available at Department)</small>
Mile Post Number: <u>3.224</u> <small>(Available at Department)</small>	Mile Post Number: _____ <small>(Available at Department)</small>

(ATTACH ADDITIONAL SHEET AS NEEDED)
CONTINUE ON NEXT PAGE

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRIVEWAY/CONNECTION APPLICATION
FOR ALL CATEGORIES

FORM 850-040-15
SYSTEMS PLANNING - 11/74
Page 4 of 6

APPLICANT (Continued)

PART 7: LOCAL GOVERNMENT DEVELOPMENT APPROVAL INFORMATION

Status of development approval (check one)

Already Approved: _____ IF SO, PLEASE ATTACH APPROVAL DOCUMENT TO APPLICATION

Approval Pending: If "approval pending" what is expected approval date? 4/97

Local government development approval official:

Name: Mr. Scott Pickett Title: Planner

Department/Office: Town of Longboat Key Planning Dept.

Street Address: 610 General Harris Street

City, State, ZIP: Longboat Key, FL 34228

Telephone: (941) 316-1966

PART 8: SITE PLAN AND CONNECTION LOCATION CHECKLIST

FOR CATEGORIES C,D,E,F & G. This is the minimum information required for review. This information shall be signed, sealed and dated by a Professional Engineer registered in Florida.

A. Site Plan Map (Scale: _____)

Number of Copies: _____

Include: N/A

- _____ Site circulation plan and parking layout
- _____ Location of your proposed connections
- _____ Location of existing median openings serving the property
- _____ Location of all public streets serving the property
- _____ All known easements
- _____ Property lines
- _____ Right of way lines
- _____ All out-parcels
- _____ Ownership of abutting parcels
- _____ Inset site location map
- _____ Location of buildings and other permanent features that affect sight distance or circulation on public roads
- _____ Existing joint property access features
- _____
- _____

Continue on next page

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRIVEWAY/CONNECTION APPLICATION
 FOR ALL CATEGORIES

FORM 850-040-15
 SYSTEMS PLANNING - 11/94
 Page 5 of 6

PART 8: SITE PLAN AND CONNECTION LOCATION CHECKLIST (CONTINUED)

<p>B. Connection Design Plan (Scale: _____)</p> <p>Includes: <u>N/A</u></p> <p>_____ Connection/driveway dimensions (include width, angle, radius, flare, etc.)</p> <p>_____ All roadway alterations</p> <p>_____ Auxiliary lanes with cross section</p> <p>_____ Existing traffic control devices</p> <p>_____ Proposed traffic control devices</p> <p>_____ Striping and signing plans</p> <p>_____ Pavement design include cross section for connection(s)</p> <p>_____ Pavement design including cross section for auxiliary lanes</p>	<p>C. Drainage from Topo Plans (Separate map from Site Plan & Connection Design may be required)</p> <p>_____ Culvert size and type</p> <p>_____ Existing grading</p> <p>_____ Proposed grading</p> <p>_____ Stormwater facilities</p> <p>_____ Drainage facilities</p> <p>_____</p> <p>_____</p> <p>_____</p>
---	---

(Due to scale, this exhibit may need to be separate from other exhibits) Recent aerial photos are acceptable.

Location and spacing of connections, median openings, and roads for: N/A

- _____ 660 Ft. (200 M) each side of the proposed connection for roads with speeds 45 MPH (70 KPH) or less
- _____ 1320 Ft. (400 M) each side of the proposed connection for roads with speeds greater than 45 MPH (70 KPH)
- _____ Above information for both sides of the street
- _____ *Other, Explain

*Due to a restrictive median, or other physical features, the requirements of this section may be less.

PART 9: TRAFFIC STUDY

FOR CATEGORY D,E,F,&G application or any application requesting a Traffic Signal, New Median Opening, or Modified Median Opening.

N/A

- _____ Traffic Study
- _____ Peak hour movements from each proposed connection
- _____ Existing conditions
- _____ Future conditions (Year _____)

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRIVEWAY/CONNECTION APPLICATION
FOR ALL CATEGORIES

FORM 850-040-15
SYSTEMS PLANNING - 11/94
Page 6 of 6

APPLICANT (Continued)

PART 10: ADDITIONAL INFORMATION WHICH MAY BE REQUESTED

- 5 Number of packages.
1 Application forms bearing original signatures.
5 Applicable designs ~~and sealed~~ and sealed by a professional engineer.

NOTES:

PART 11: NOTICE TO APPLICANT

Proposed features in the right-of-way, such as median openings and other traffic control devices, are not part of the connection(s) to be authorized by a connection permit. The Department reserves the right to change these features in the future in order to promote safety in the right of way or efficient traffic operations on the highway. Expenditure by the applicant of monies for installation or maintenance of such features shall grant no vested right in the maintenance of such features.

PART 12: CERTIFICATION AND SIGNATURE

I certify that I am familiar with the information contained in this application and that to the best of my knowledge and belief such information is true, complete and accurate.

Signed _____
(Applicant or authorized representative)

Date: _____

Printed Name: _____

TITLE: _____

William B. Houghton, P.E. 33150
(PROFESSIONAL ENGINEER REGISTRATION NUMBER (IF APPLICABLE))

5940 Palmer Blvd.
(Mailing Address)

Sarasota, FL 34232
(City) (State) Zip

END

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRIVEWAY CONNECTION PERMIT
FOR ALL CATEGORIES

FORM 850-040-18
SYSTEMS PLANNING
11/93
Page 1 of 3

PART 1: PERMIT INFORMATION

APPLICATION NUMBER: _____
Permit Category: B Access Classification: _____
PROJECT: Wake Island Road (20' alley)
PERMITTEE: Town of Longboat Key
Section/Mile Post: 13080/3.224 State Road: 789
Section/Mile Post: _____ State Road: _____
Section/Mile Post: _____ State Road: _____

PART 2: PERMITTEE INFORMATION

Permittee Name: Town of Longboat Key
Permittee Mailing Address: 501 Bay Isles Road
City, State, Zip: Longboat Key, FL 34228
Telephone: (941) 316-1999
Engineer/Consultant/Project Manager: _____
Engineer responsible for construction inspection: William B. Houghton 33150
Mailing Address: 5940 Palmer Blvd. NAME P.E.#
City, State, Zip: Sarasota, FL 34232
Telephone: (941) 379-8831 (_____)
(941) 379-0556 (_____) Mobile or FAX Phone: _____
(CIRCLE ONE)

PART 3: PERMIT APPROVAL

The above application has been reviewed and is hereby approved subject to all Provisions as attached.

PERMIT NUMBER: _____
Department of Transportation

Construction shall begin by: _____
BY: _____
(Date)

and shall be completed no later than: _____ TITLE: _____
(Date)

Special provisions attached YES NO
Date of Issuance: _____

NOTE: This permit is only valid for one calendar year from date of issuance.

See reverse side for General and Special Provisions

PART 4: GENERAL PROVISIONS

1. Notify the Department of Transportation Maintenance Office at least 48 hours in advance of starting proposed work.
Phone: _____, Attention: _____
2. A copy of the approved permit must be displayed in a prominent location in the immediate vicinity of the connection construction.
3. Comply with 14-96.008(7) on Utility Notification Requirements.
4. Comply with 14-96.008 Construction & Maintenance of Traffic Requirements.
5. All work performed in the Department's right of way shall be done in accordance with the most current Department standards, specifications and the permit provisions.
6. The permittee shall not commence use of the connection prior to a final inspection and acceptance by the Department.
7. Comply with 14-96.003(4)(a) Cost of Construction.
8. If a Significant Change of the permittee's land use, as defined in Section 335.18, Florida Statutes, occurs, the Department may begin the process to modify or revoke the connection permit.
9. Medians may be added and median openings may be changed by the Department as part of a Construction Project or Safety Project. The provision for a median might change the operation of the connection to be for right turns only.
10. All conditions in NOTICE OF INTENT WILL APPLY unless specifically changed by the Department.
11. All approved connection(s) and turning movements are subject to the Department's continuing authority to revoke or modify such connection(s) or turning movements in order to protect safety and traffic operations on the state highway or State Highway System.
12. **Transportation Control Features in the state right of way.** Transportation control features in the Department's right of way, including, but not limited to, traffic signals, medians, median openings, or any other transportation control features or measures in the state right of way are operational and safety characteristics of the State Highway and are not means of access. The Department may install, remove or modify any present or future transportation control feature in the state right of way such as median opening, traffic control device or a feature affecting turning movements through a connection, to make changes to promote safety in the right of way or efficient traffic operations on the highway.
13. The Permittee for him/herself, his/her heirs, his/her assigns and successors in interest, binds and is bound and obligated to save and hold the State of Florida, The Department, its agents and employees harmless from any and all damages, claims, expense, or injuries arising out of any act, neglect or omission by the applicant, his/her heirs, assigns and successors in interest that may occur by reason of this, facility design, construction, maintenance or continuing existence of the connection facility, except that the applicant shall not be liable under this provision for damages arising from the sole negligence of the Department.
14. The Permittee shall be responsible for determining and notify all other users of the right of way.

PART 5: SPECIAL PROVISIONS

NON-CONFORMING CONNECTIONS: YES
 NO

If this is a non-conforming connection permit, as defined in Rule Chapters 14-96 and 14-97, then the following may be a part of this permit:

1. The non-conforming connection(s) described in this permit will carry no more than _____ in average daily traffic, or _____ in peak hour traffic.
2. All non-conforming connections will be subject to closure or relocation when reasonable access becomes available in the future.

OTHER SPECIAL PROVISIONS

PART 6: PERMITTEE CERTIFICATION AND SIGNATURE

This permit is hereby signed and all provisions agreed to on date: _____

I certify that I have read and understand the information and provisions contained in this permit. I further certify that I possess the authority to undertake the proposed activities.

WITNESSED BY: _____
(Signature)

Signed: _____
(Applicant or authorized representative)

PRINT OR TYPE: _____

PRINT OR TYPE: _____

(Mailing Address)

TITLE: _____

(City) (State) (Zip)

(City) (State) (Zip)

DATE: _____

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

EXHIBIT C-1

**ARCHITECT'S PLANS
(ATTACHED)**

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE

EXHIBIT D-1

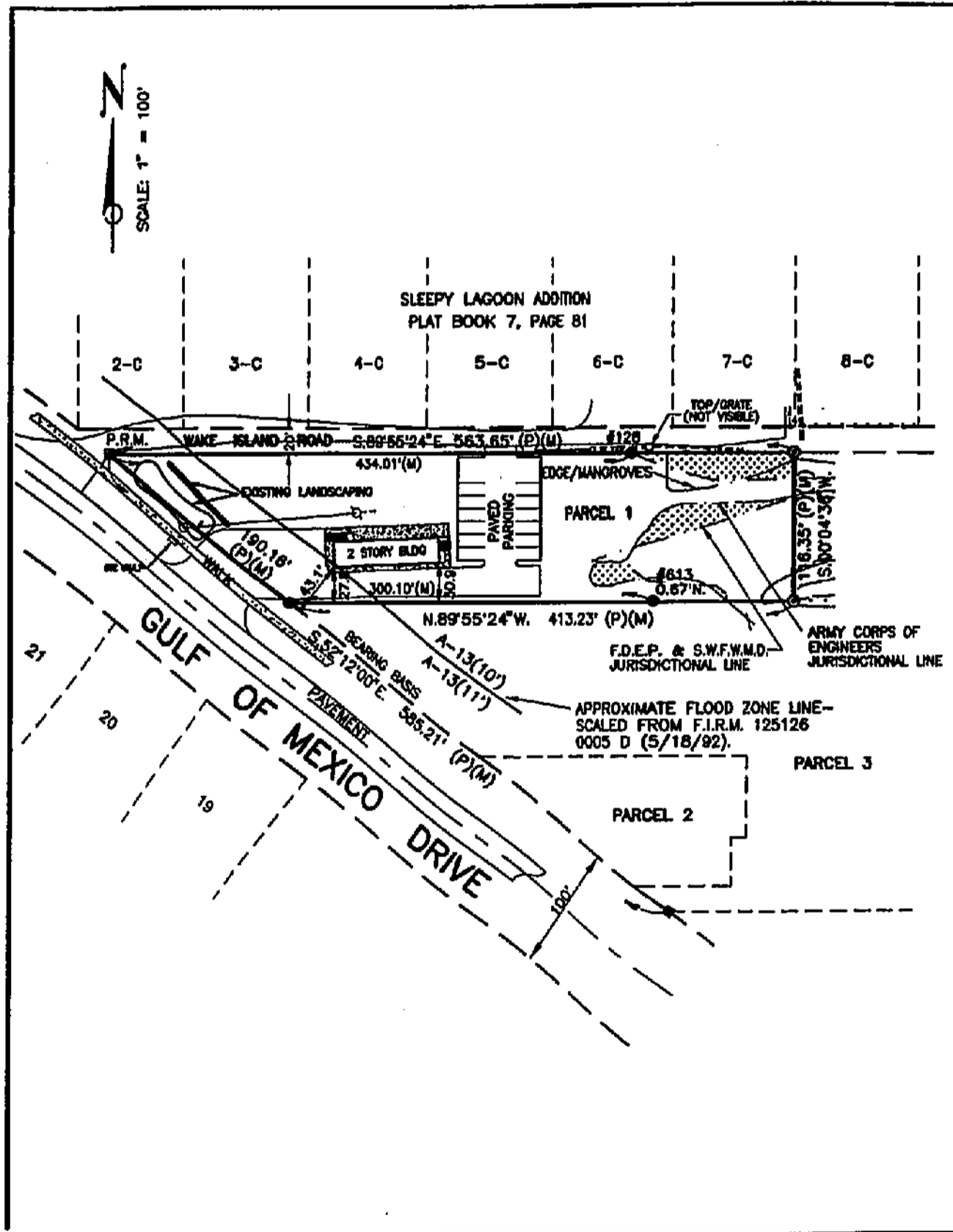
SCHEDULE OF CONSTRUCTION

EXHIBIT D-1
SCHEDULE OF CONSTRUCTION

Construction will be in accordance with 158.099 (F)

- 1. Within 12 months of the date of approval of an application for site plan review, a complete application for building permit will have been submitted to the Town and a building permit issued.**
- 2. A final certificate of occupancy for all phases of the project will have been issued within three years from the date set for receipt of a complete application for building permit for the final building or development phase of the project.**

HFI DEVELOPMENT INC
QUASIJUDICIAL FILE



BOUNDARY SURVEY

PARCEL 1, LAGUNA AT LONGBOAT KEY, A SUBDMISION, AS RECORDED IN PLAT BOOK 28, PAGES 160, 161 AND 162, PUBLIC RECORDS OF MANATEE COUNTY, FLORIDA.

LEGEND

- (P) PLAT
- (M) MEASURED
- L.B. LAND SURVEYOR BUSINESS
- P.L.S. PROFESSIONAL LAND SURVEYOR
- ⊙ SET 5/8" IRON ROD W/CAP - L.B.#6481
- FOUND IRON ROD W/CAP - L.B.#6432
- FOUND CONCRETE MONUMENT
- P.R.M. PERMANENT REFERENCE MONUMENT
- ⊘ UTILITY POLE

SURVEYORS NOTES

1. UNDERGROUND FEATURES (I.E. DRAINAGE PIPES, UTILITY LINES, FOUNDATIONS, IRRIGATION LINES, ETC.) ARE NOT ABSTRACTED AS PART OF THIS SURVEY.
2. BEARINGS BASED ON PLAT LINE AS SHOWN.
3. SURVEYED PARCEL CONTAINS 56,829 ± SQ.FT. (1.30 ACRES)

CERTIFICATION

I HEREBY CERTIFY THAT THIS SURVEY MEETS THE MINIMUM TECHNICAL STANDARDS AS SET FORTH BY THE FLORIDA BOARD OF PROFESSIONAL LAND SURVEYORS IN CHAPTER 61G17-6, FLORIDA ADMINISTRATIVE CODE, PURSUANT TO SECTION 472.027, FLORIDA STATUTES.

Alex S. Esber
ALEX S. ESBER
PROFESSIONAL LAND SURVEYOR
FLORIDA CERTIFICATE #4349

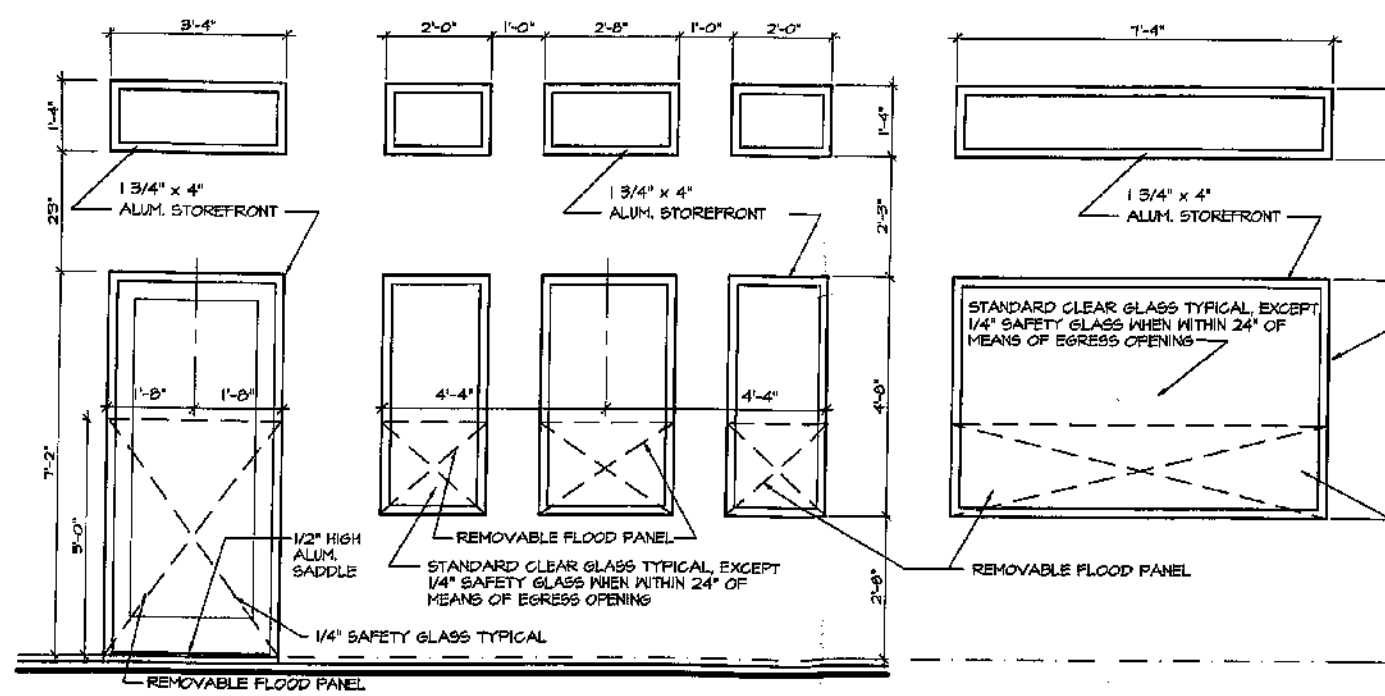
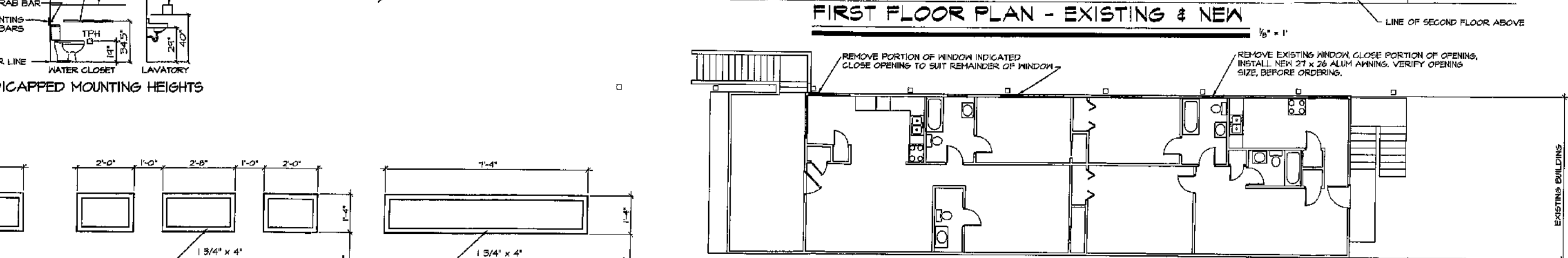
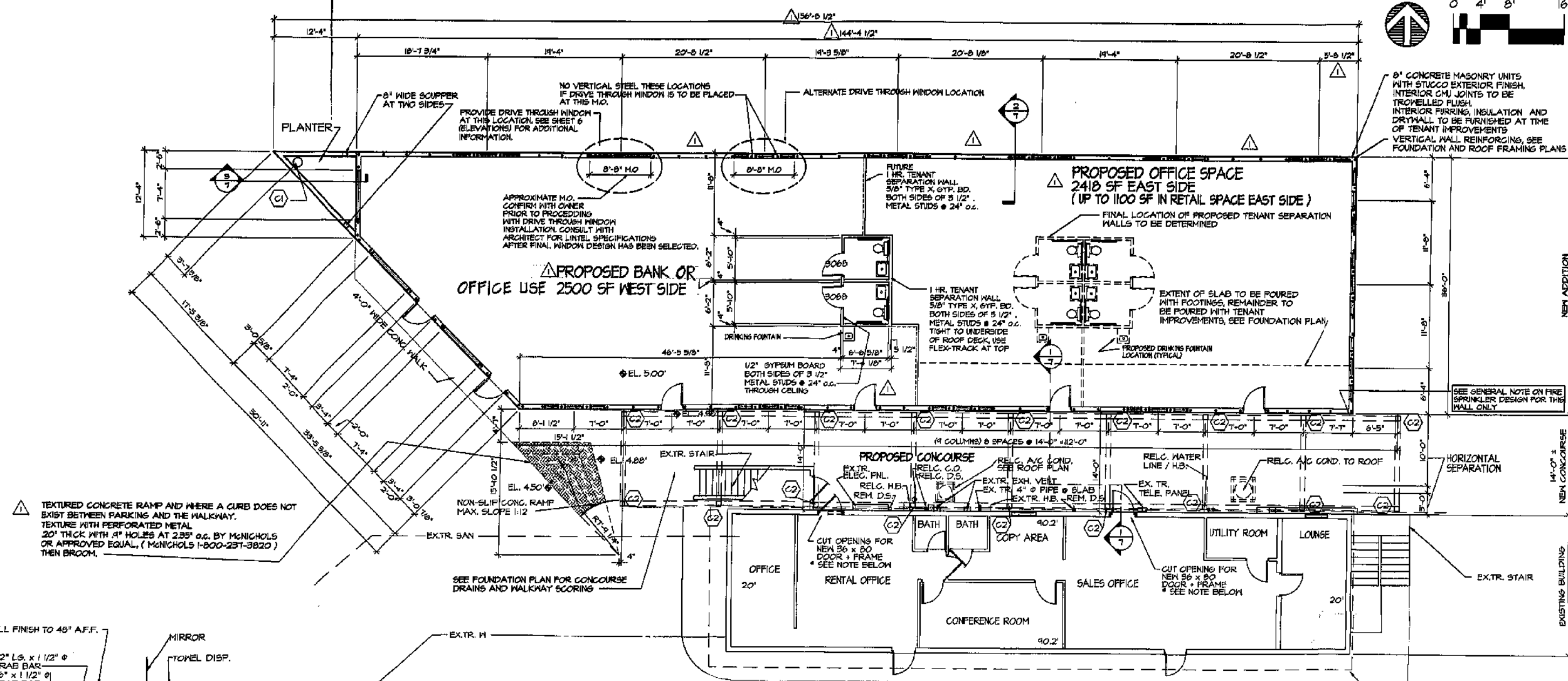
APR 28 1995
SIGNING DATE
4/14/95
FIELD SURVEY DATE

Landry & Esber
ENGINEERS - SURVEYORS

130 N. Tarnham Trail, Suite 301 Sarasota, Florida 34236
(813)955-6004 FAX (813)955-7501

REVISED			
BGR	5/31/95	SURVEY (WEDEBND.DWG)	B02502/55
BY	DATE	DESCRIPTION	

* NOT VALID UNLESS SIGNED AND STAMPED WITH EMBOSSED SEAL.



TYP. EXTERIOR DOOR / WINDOW / FLOOD PANEL AND TRANSOM
1/2" = 1'-0" SEE PLAN AND ELEVATION DRAWINGS FOR LOCATIONS

PLANS VALID FOR PERIOD OF 90 DAYS WITHOUT SIGNATURE AND/OR RENEWAL SEAL

SKIRBALL GROUP, INC.
ARCHITECTS PLANNERS
1508 S. ORANGE AVE.
GAINESVILLE, FL 32608
(813) 351-1777
FAX (813) 354-0308
LIC. NO. AAC001848

COPYRIGHT 1998
DESIGNED BY HFI DEVELOPMENT INC. EXPRESSLY RESERVES HIS COPYRIGHT AND OTHER RIGHTS IN THESE PLANS. THESE PLANS AND DRAWINGS ARE NOT TO BE REPRODUCED, CHANGED OR COPIED IN ANY FORM OR MANNER WITHOUT THE WRITTEN CONSENT OF HFI DEVELOPMENT INC. ANY REPRODUCTION OR CHANGE WITHOUT THE WRITTEN CONSENT OF HFI DEVELOPMENT INC. SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO HFI DEVELOPMENT INC. THESE PLANS ARE NOT TO BE USED FOR ANY OTHER PROJECT WITHOUT THE WRITTEN CONSENT OF HFI DEVELOPMENT INC. THESE PLANS ARE NOT TO BE CONTINUED UNLESS THE SQUARE TO THE LEFT OF THE WORD 'CONSTRUCTION' BELOW IS DRAINED IN.

COMMERCIAL DEVELOPMENT FOR HFI DEVELOPMENTS INC.

FLOOR PLANS

■ SCHEMATIC	■
■ DEVELOPMENT	■
■ BIDDING	■
■ CONSTRUCTION	■
PROJECT NO.	4550
DATE	2-16-98
REVISIONS:	
TOILET R.M.S. ADDED	2-26-98
REVIEW COMMENTS (05/01/97)	5-8-98
(DRIVE THROUGH 05/01/97)	

DRAWING NO.
3 OF 11

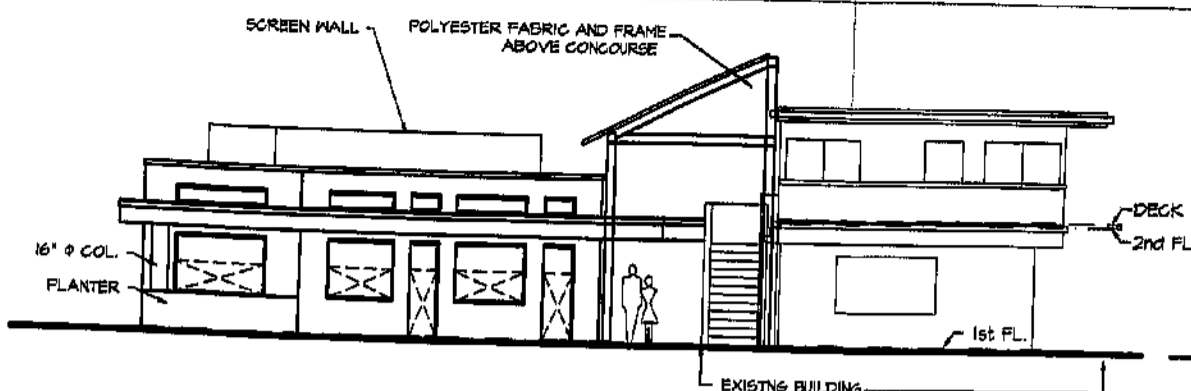
NOTE: ALL FIXTURES AND FITTINGS NOT IDENTIFIED SHALL REMAIN IN SERVICE. RELOCATE AS MAY BE NECESSARY TO MAINTAIN INTEGRITY AND USABILITY OF EXISTING STRUCTURE. CONTACT ARCHITECT BEFORE PROCEEDING WITH ANY ALTERATIONS REGARDING SAME.

EX. TR. (ITEM) = EXISTING TO REMAIN (ITEM)
 REM. (ITEM) = REMOVE EXISTING (ITEM) w/ ACCESSORIES
 RELC. (ITEM) = RELOCATE (ITEM) AS REQUIRED

*** NOTE:** PROVIDE PRECAST CONC. LINTEL AND FLOOD PANEL FOR NEW DOOR OPENINGS IN EXISTING BUILDING. RESTORE EXTERIOR & INTERIOR FACES OF WALL TO LIKE NEW CONDITION AT NEW OPENINGS

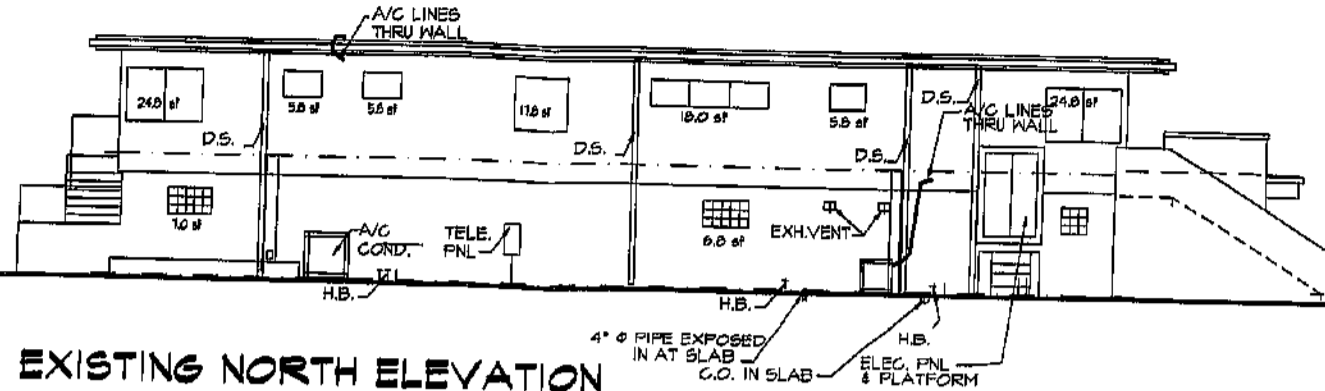
GENERAL NOTE (FIRE SPRINKLERS)
 GENERAL CONTRACTOR TO SUBMIT FOR FIRE MARSHALL'S APPROVAL FLORIDA P.E. SIGNED & SEALED SHOP DRAWINGS WITH SIDEWALL MOUNTED SPRINKLER HEADS LOCATED AS NOTED.

GENERAL NOTE:
 THIS APPLICATION FOR SITE PLAN AMENDMENT IS BEING FILED BY THE APPLICANT SOLELY AND EXCLUSIVELY TO EXHAUST ALL OF THE APPLICANT'S ADMINISTRATIVE REMEDIES AND TO OBTAIN A FINAL DECISION BY THE TOWN OF LONGBOAT KEY, FLORIDA, ON A WITH RESPECT TO THE APPLICANT'S LAND USE AND DEVELOPMENT REQUEST. THIS APPLICATION IS BEING FILED BY THE APPLICANT WITH AN ABSOLUTE AND COMPLETE RESERVATION OF ALL THE APPLICANT'S RIGHTS, REMEDIES, DEFENSES, CLAIMS AND CAUSES OF ACTION.



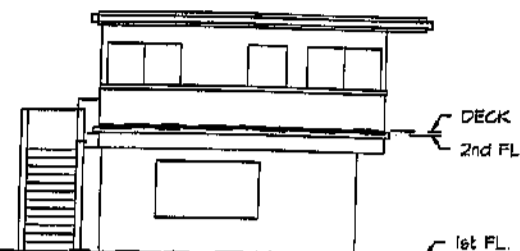
ADDITION - WEST ELEVATION - EXISTING

1/8" = 1'



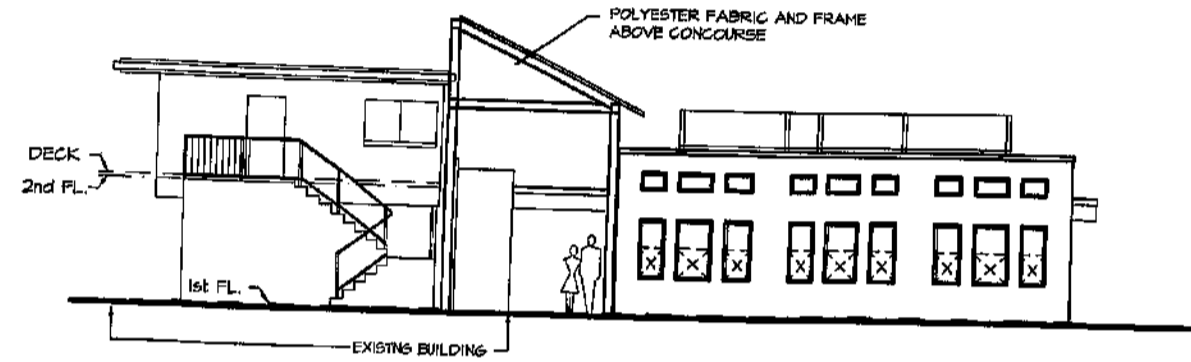
EXISTING NORTH ELEVATION

1/8" = 1'



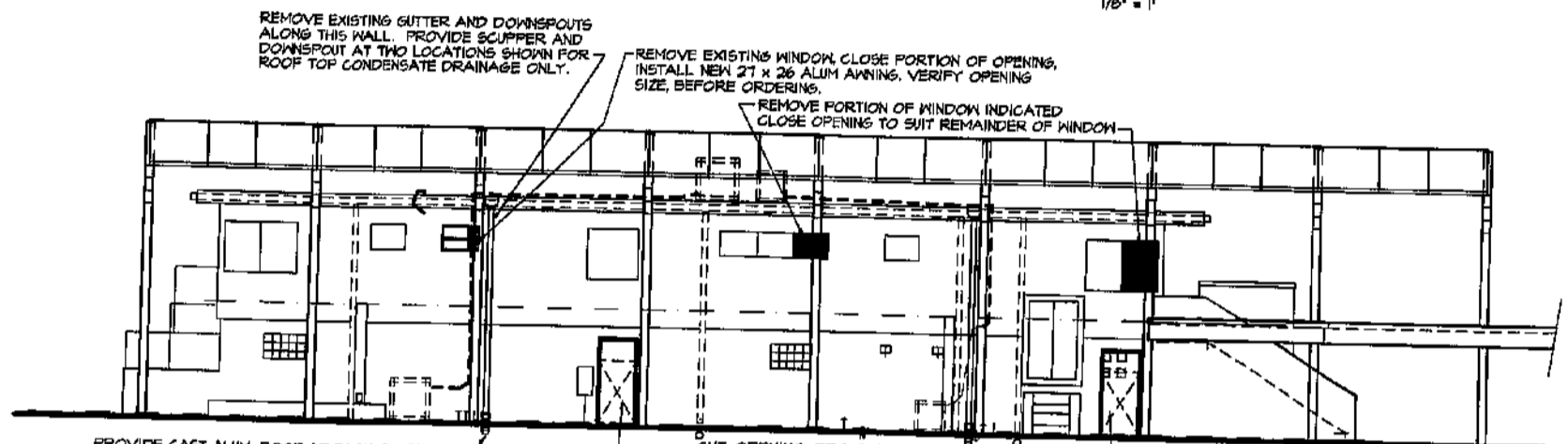
EXISTING WEST ELEVATION

1/8" = 1'



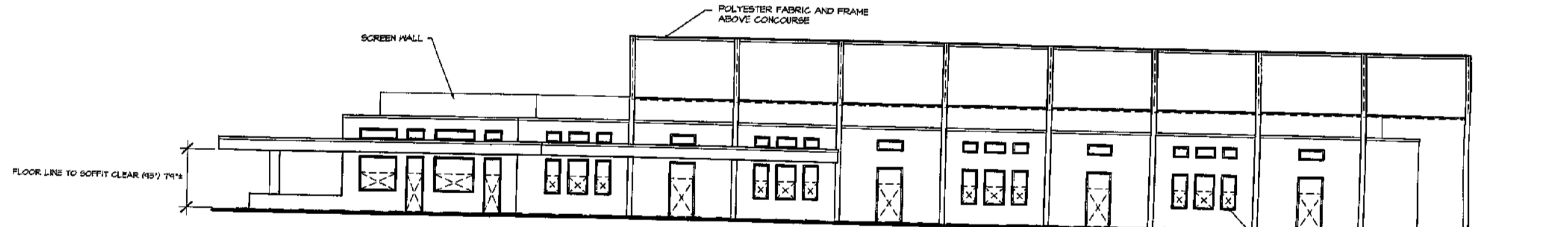
EXISTING - EAST ELEVATION - ADDITION

1/8" = 1'



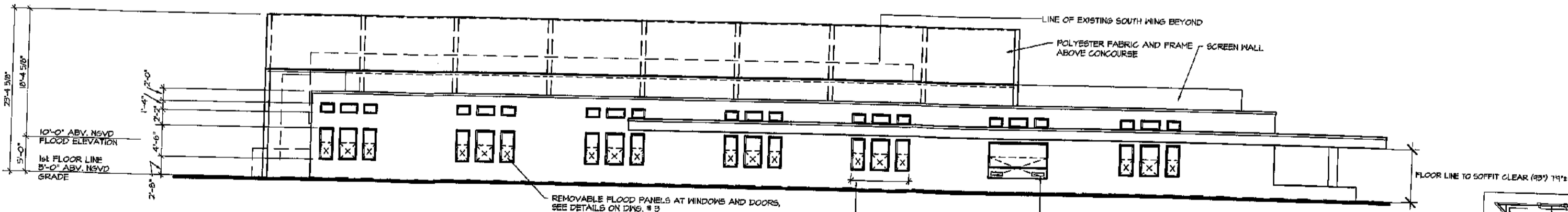
EXISTING NORTH ELEVATION W/ CONCOURSE FENESTRATION

1/8" = 1'



ADDITION - SOUTH ELEVATION

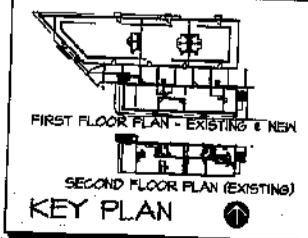
1/8" = 1'



NORTH ELEVATION - ADDITION

1/8" = 1'

GENERAL NOTE:
THIS APPLICATION FOR SITE PLAN AMENDMENT IS BEING FILED BY THE APPLICANT SOLELY AND EXCLUSIVELY TO EXHAUST ALL OF THE APPLICANT'S ADMINISTRATIVE REMEDIES AND TO OBTAIN A FINAL DECISION BY THE TOWN OF LONGBOAT KEY, FLORIDA, ON REQUEST. THIS APPLICATION IS BEING FILED BY THE APPLICANT WITH AN ABSOLUTE AND COMPLETE RESERVATION OF ALL THE APPLICANT'S RIGHTS, REMEDIES, DEFENSES, CLAIMS AND CAUSES OF ACTION.



DRAWING NO.

6 OF 11

PLAN NOT VALID FOR BUILDING PERMIT WITHOUT SIGNATURE AND/OR SEALED SEAL

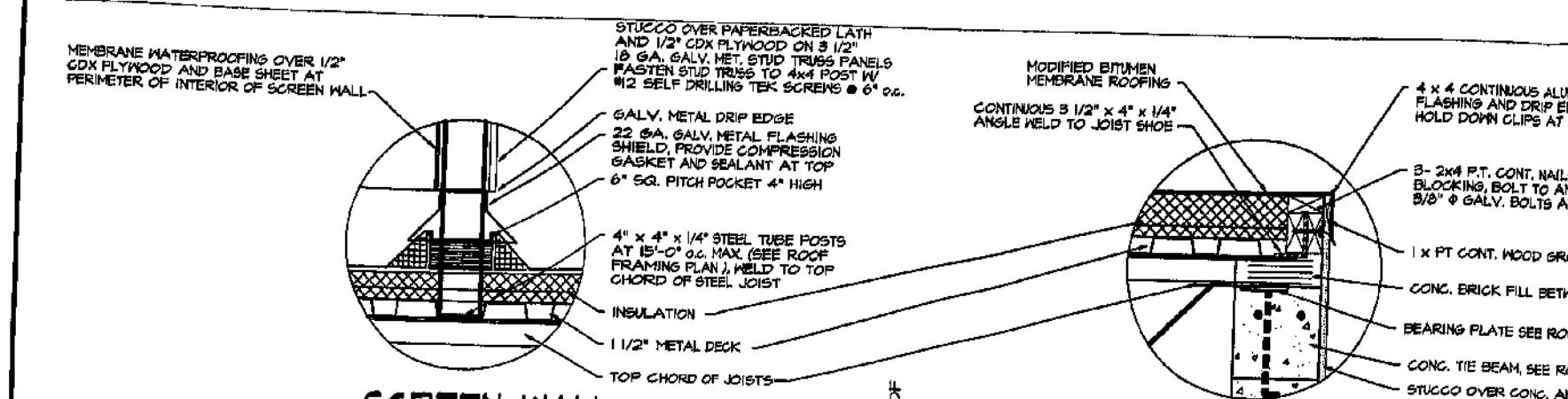
SKIRBALL GROUP, INC.
ARCHITECTS PLANNERS
1208 S. ORANGE AVE.
SARASOTA, FL 34239
(841) 951-1777
FAX (841) 954-0208
LIC. NO. AAC001848

© COPYRIGHT 1998
DESIGNER HEREBY EXPRESSLY RESERVES HIS COPYRIGHT AND OTHER RIGHTS IN THESE PLANS. THESE PLANS AND DRAWINGS ARE NOT TO BE REPRODUCED, COPIED OR COPIED IN ANY FORM OR MANNER WHATSOEVER WITHOUT THE WRITTEN PERMISSION OF THE DESIGNER. PERMISSION AND TO BE ASSIGNED TO ANY PARTY WITHOUT FIRST OBTAINING THE WRITTEN PERMISSION AND CONSENT OF THE DESIGNER. THESE PLANS ARE NOT TO BE USED FOR ANY OTHER PROJECT OTHER THAN THE PROJECT SPECIFICALLY IDENTIFIED ON THESE PLANS. THESE PLANS ARE NOT TO BE USED FOR CONSTRUCTION UNLESS THE SQUARE TO THE LEFT OF THE WORD "CONSTRUCTION" BELOW IS CHECKED IN.

COMMERCIAL DEVELOPMENT FOR HFI DEVELOPMENTS INC.

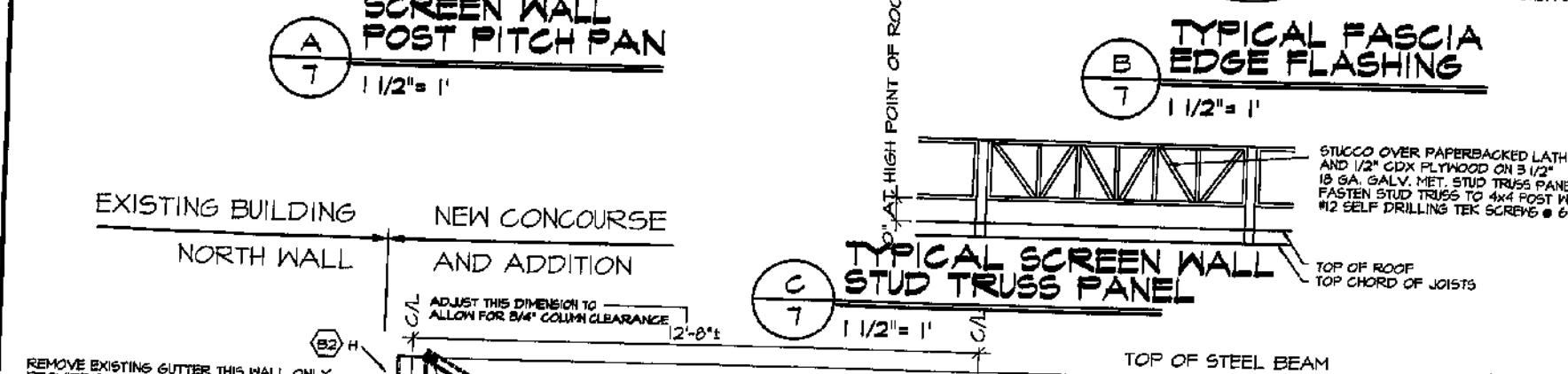
ELEVATIONS

<input checked="" type="checkbox"/>	SCHEMATIC	<input type="checkbox"/>
<input checked="" type="checkbox"/>	DEVELOPMENT	<input type="checkbox"/>
<input checked="" type="checkbox"/>	BIDDING	<input type="checkbox"/>
<input type="checkbox"/>	CONSTRUCTION	<input type="checkbox"/>
PROJECT NO. 4530		
DATE 2-16-98		
REVISIONS:		
1. 04/04/01 REVIEW COMMENTS (04/04/01 DRIVE THROUGH)		

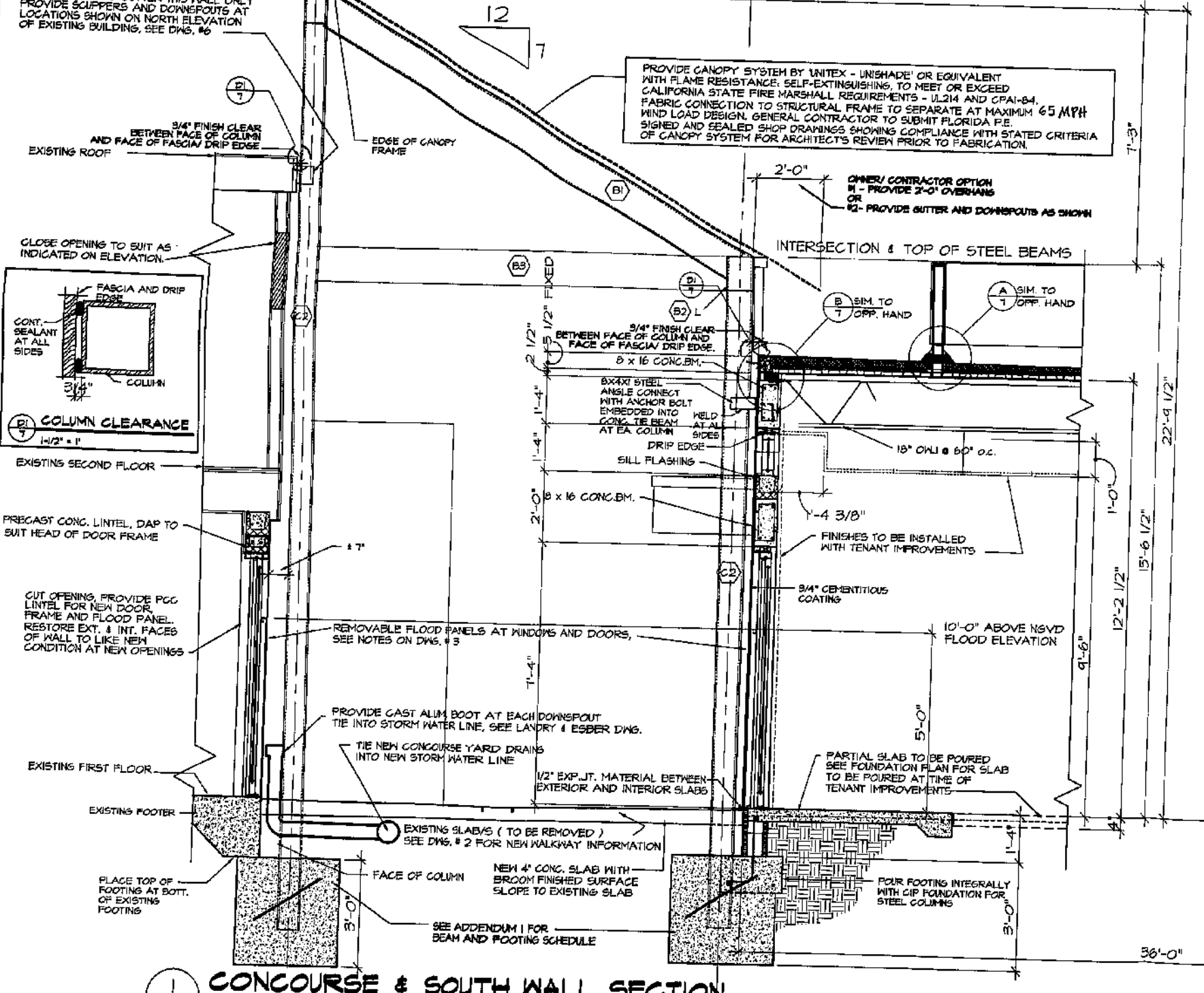


SCREEN WALL POST PITCH PAN
1 1/2" = 1"

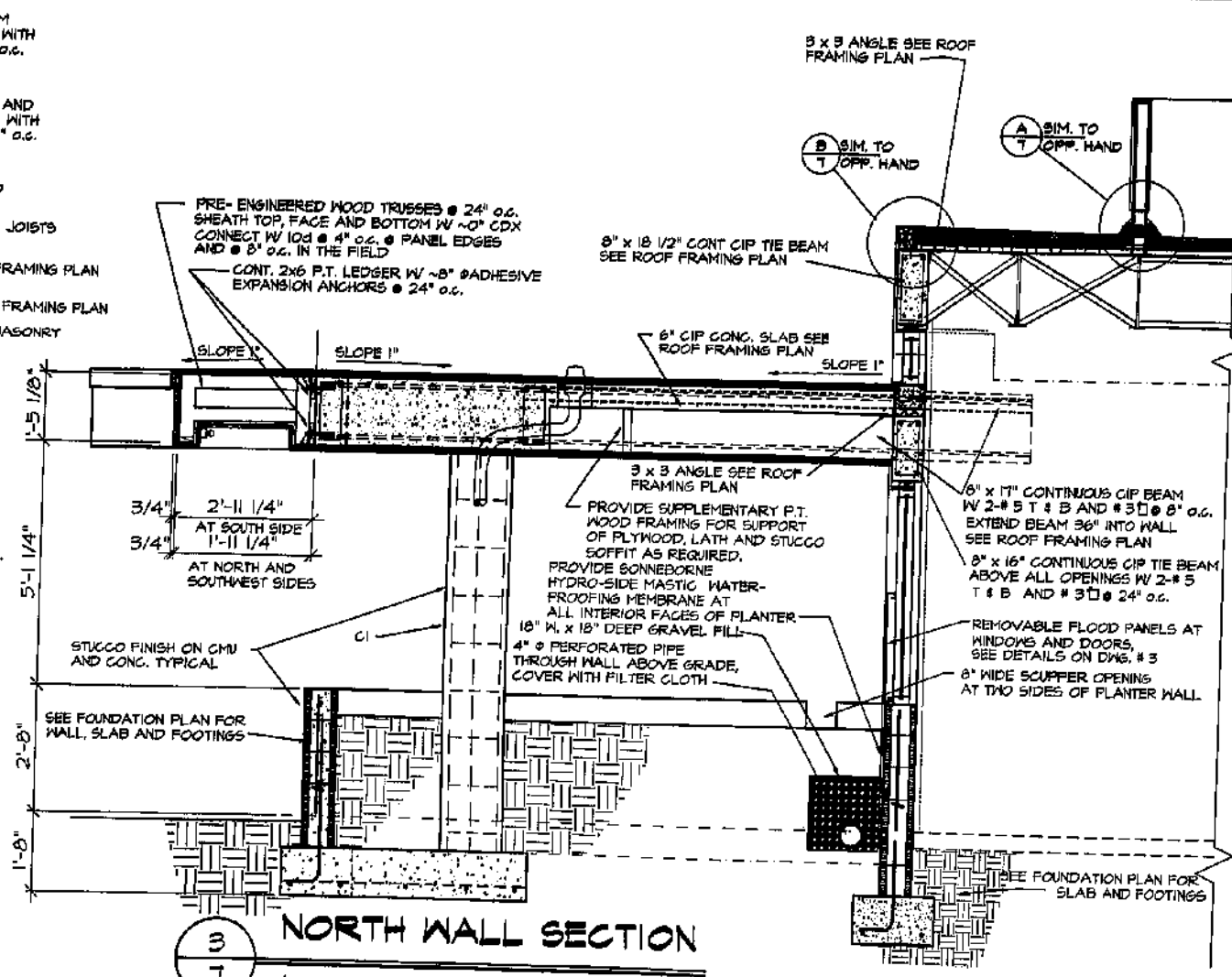
TYPICAL FASCIA EDGE FLASHING
1 1/2" = 1"



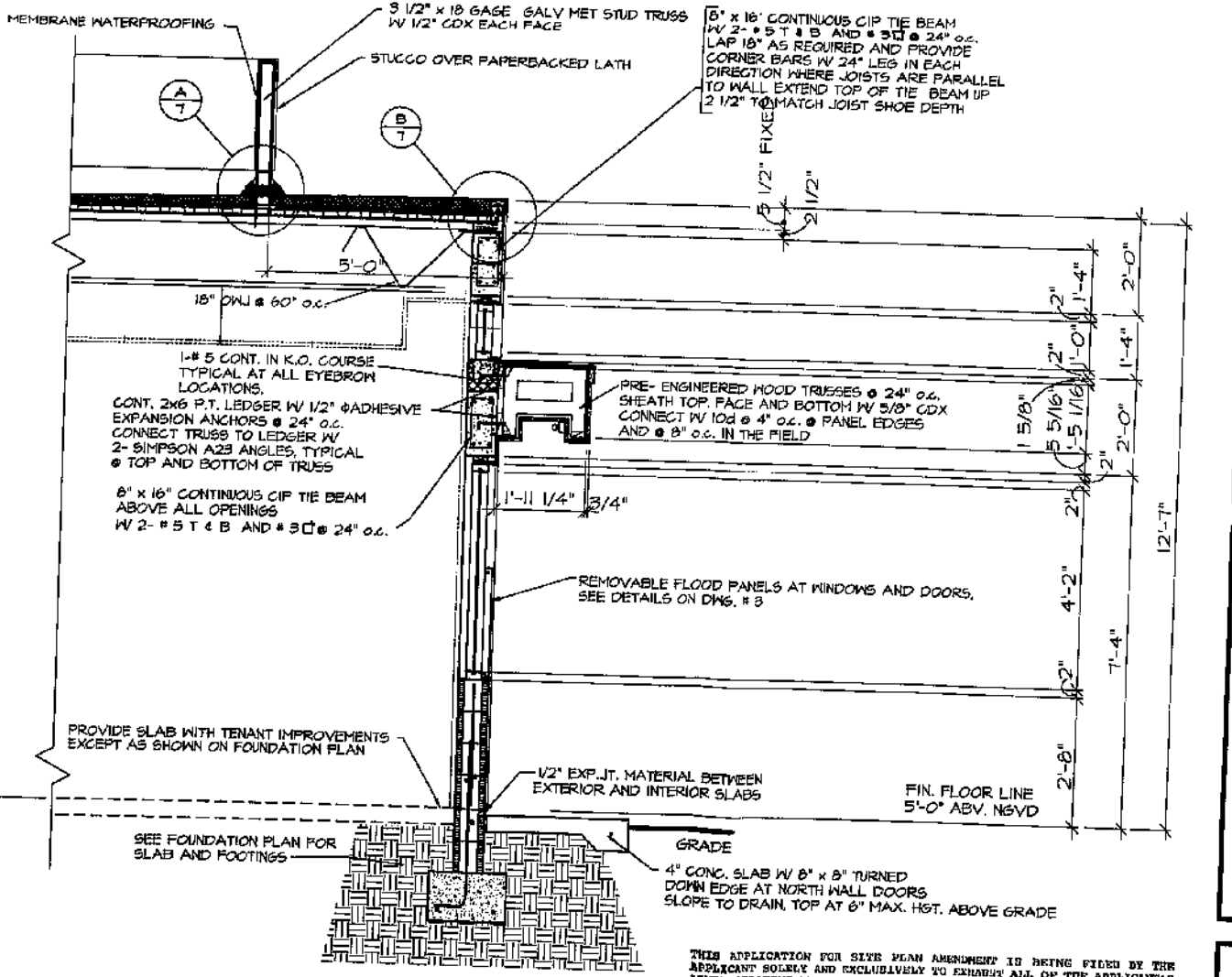
TYPICAL SCREEN WALL STUD TRUSS PANEL
1 1/2" = 1"



CONCOURSE & SOUTH WALL SECTION
1/2" = 1"



NORTH WALL SECTION
1/2" = 1"



NORTH WALL SECTION
1/2" = 1"

PLAN NOT VALID FOR
CONSTRUCTION WITHOUT
SIGNATURE AND/OR
STAMP

SKIRBALL GROUP, INC.
ARCHITECTS
PLANNERS
1908 S. ORANGE AVE.
SARASOTA, FL. 34238
(841) 561 1777
FAX (841) 564 0308
LIC. NO. A4000248

COPYRIGHT 1996
DESIGNER HEREBY EXPRESSLY RESERVES HIS
COPYRIGHT AND OTHER RIGHTS IN THESE PLANS.
REPRODUCTION, COPIED OR NOT TO BE,
OR MAKING REPRODUCTIONS IN ANY FORM
OR BY ANY MEANS WITHOUT THE WRITTEN
CONSENT OF THE DESIGNER IS PROHIBITED AND
CONSIDERED TO BE A VIOLATION OF FEDERAL
AND STATE LAWS. THESE PLANS ARE NOT TO
BE USED FOR ANY OTHER PROJECT WITHOUT THE
WRITTEN CONSENT OF THE DESIGNER. THESE
PLANS ARE NOT TO BE USED FOR ANY OTHER
PROJECT WITHOUT THE WRITTEN CONSENT OF
THE DESIGNER. THE WORD "CONSTRUCTION" BELOW IS
STRIKED IN.

**COMMERCIAL
DEVELOPMENT
FOR
HFI
DEVELOPMENTS
INC.**

**WALL
SECTIONS
AND DETAILS**

SCHEMATIC
 DEVELOPMENT
 BIDDING
 CONSTRUCTION

PROJECT NO. 4530
DATE 2-16-96

REVISIONS:

04/04/97 REVISION COMMENTS

DRAWING NO.
7 OF 11

C:\ARCH\96\23-1\WALLS.EC: Thu Jan 10 17:00:21 1997 COPYRIGHT 1997 SKIRBALL GROUP, INC.

HFI DEVELOPMENTS, INC.

SITE PLAN AMENDMENT

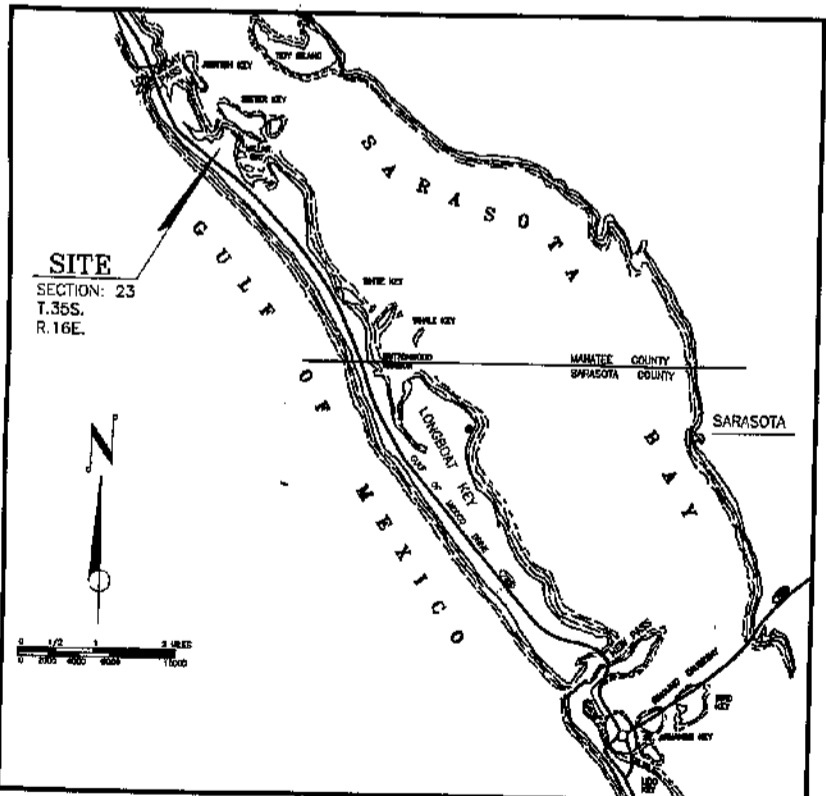
GENERAL NOTES

- CAUTION**
THE LOCATION OF EXISTING UTILITIES SHOWN ON THESE PLANS ARE APPROXIMATE. PRIOR TO THE COMMENCEMENT OF EXCAVATION, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE EXACT LOCATION OF UNDERGROUND UTILITIES BY CONTACTING:
TOWN OF LONGBOAT KEY UTILITIES
MANATEE CO. UTILITIES DEPARTMENT
FLORIDA POWER & LIGHT
GENERAL TELEPHONE COMPANY OF FLORIDA
STORER CABLE TV
FIRST CALL 1-800-227-5385 48 HOURS BEFORE YOU DIG
CANDY 1-800-282-8981
- RESPONSIBILITY REGARDING EXISTING UTILITIES & STRUCTURES**
THE EXISTENCE AND LOCATION OF EXISTING UNDERGROUND UTILITIES ARE NOT GUARANTEED AND SHALL BE INVESTIGATED AND VERIFIED IN THE FIELD BY THE CONTRACTOR BEFORE STARTING WORK. EXCAVATION IN THE VICINITY OF EXISTING STRUCTURES AND UTILITIES SHALL BE CAREFULLY DONE. BY HAND IF NECESSARY, THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY DAMAGE TO, AND FOR MAINTENANCE AND PROTECTION OF, EXISTING UTILITIES AND STRUCTURES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE POINT OF CONNECTION OF ALL BUILDING SERVICE LINES AT THE BUILDING, PRIOR TO INSTALLATION.**
- EROSION CONTROL DURING CONSTRUCTION**
THE CONTRACTOR SHALL TAKE ALL NECESSARY MEASURES TO PREVENT THE RUNOFF OF ERODED SOIL MATERIAL FROM LEAVING THE CONSTRUCTION SITE AND BEING DEPOSITED ON THE PROPERTY OF OTHERS OR ON PORTIONS OF THE SITE OUTSIDE THE CONSTRUCTION LIMITS.
- THE CONTRACTOR SHALL INSURE THAT ALL NECESSARY PERMITS ARE OBTAINED PRIOR TO COMMENCING ANY CONSTRUCTION ACTIVITY.**
- LAWS TO BE OBSERVED**
THE CONTRACTOR IS ASSUMED TO BE FAMILIAR WITH, AND AT ALL TIMES SHALL OBSERVE AND COMPLY WITH, ALL FEDERAL, STATE AND LOCAL LAWS, BYLAWS, SAFETY LAWS, BUILDING AND CONSTRUCTION

LANDRY & ESBER
Engineers • Surveyors

5940 Palmer Blvd. • Sarasota, FL 34232
(941)379-8831 • Fax (941)379-0556

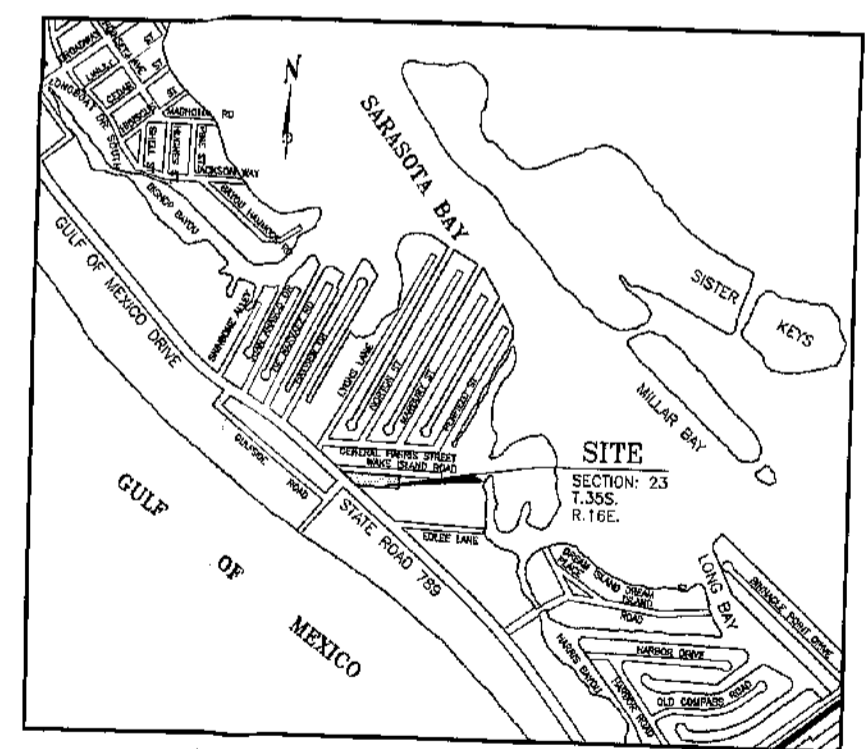
THIS APPLICATION FOR SITE PLAN AMENDMENT IS BEING FILED BY THE APPLICANT SOLELY AND EXCLUSIVELY TO EXHAUST ALL OF THE APPLICANT'S ADMINISTRATIVE REMEDIES AND TO OBTAIN A FINAL DECISION BY THE TOWN OF LONGBOAT KEY, FLORIDA, ON AND WITH RESPECT TO THE APPLICANT'S LAND USE AND DEVELOPMENT REQUEST. THIS APPLICATION IS BEING FILED BY THE APPLICANT WITH AN ABSOLUTE AND COMPLETE RESERVATION OF ALL OF THE APPLICANT'S RIGHTS, REMEDIES, DEFENSES, CLAIMS AND CAUSES OF ACTION.



VICINITY MAP

INDEX
PROJECT NO: B02502

SHEET NO.	DESCRIPTION
D-1	COVER SHEET
D-2	EXISTING CONDITIONS
D-3	AERIAL PHOTOGRAPH
D-4	SITE PLAN AMENDMENT
D-5	SITE DATA
D-6	TREE LOCATION PLAN
D-7	LANDSCAPING AND STABILIZATION PLAN



LOCATION MAP

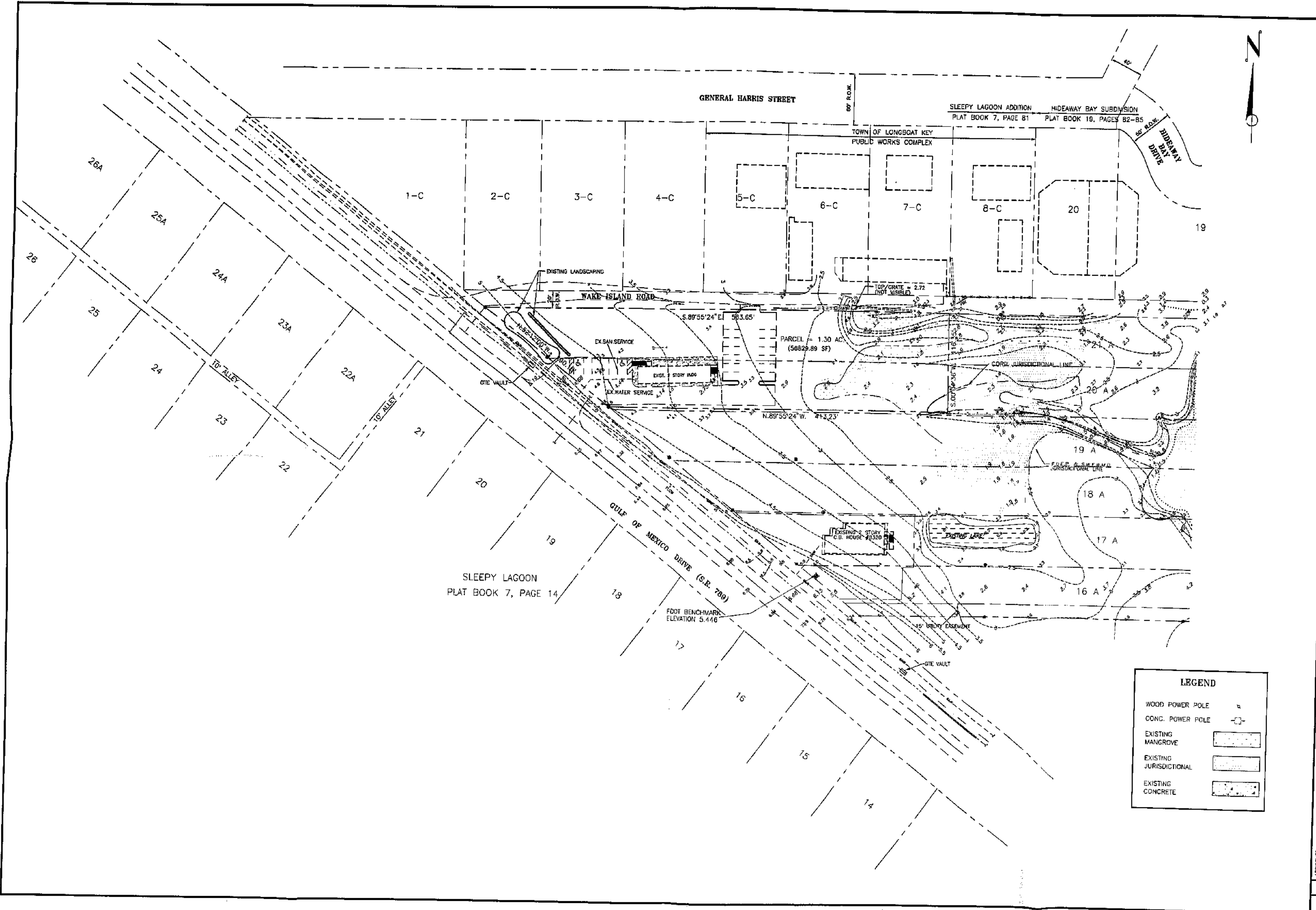
APPROVED: _____
ENGINEER OF RECORD _____ DATE _____

HFI DEVELOPMENTS, INC.
SITE PLAN AMENDMENT
COVER SHEET

DATE: 01/27/08	BY: _____
SCALE: _____	DESCRIPTION: _____
PROJECT NO: B02502	DATE: _____
SHEET NO: D-1	DATE: _____

LANDRY & ESBER
Engineers • Surveyors
 5940 Palmer Blvd. • Sarasota, FL 34232
 (941)379-8831 • Fax (941)379-0556

APPROVED: _____
 ENGINEER OF RECORD _____ DATE _____
 NOT ISSUED FOR CONSTRUCTION



LEGEND

WOOD POWER POLE	
CONC. POWER POLE	
EXISTING MANGROVE	
EXISTING JURISDICTIONAL	
EXISTING CONCRETE	

HFI DEVELOPMENTS, INC.
SITE PLAN AMENDMENT
EXISTING CONDITIONS

DATE	BY	DATE	DESCRIPTION

LANDRY & ESBER
Engineers - Surveyors
5940 Palmer Blvd. - Sarasota, FL 34232
(941) 379-8881 - Fax (941) 379-0566

APPROVED: _____
ENGINEER OF RECORD DATE _____
NOT ISSUED FOR CONSTRUCTION

PROJECT NO: B02505
SHEET NO: 0-2



LANDRY & ESBER
Engineers & Surveyors
1100 N. GULF DR. SUITE 100
DUNEDIN, FL 34628

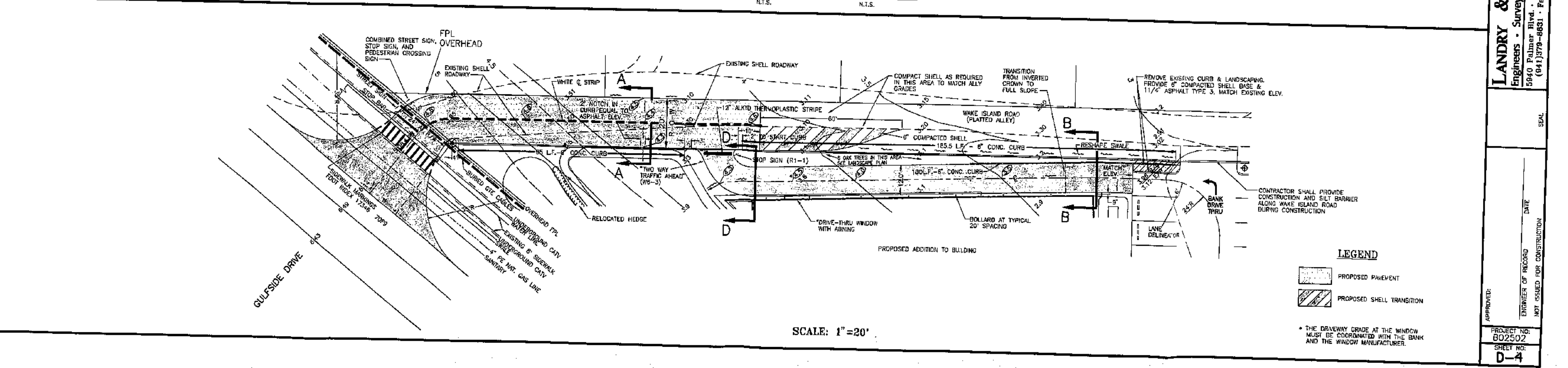
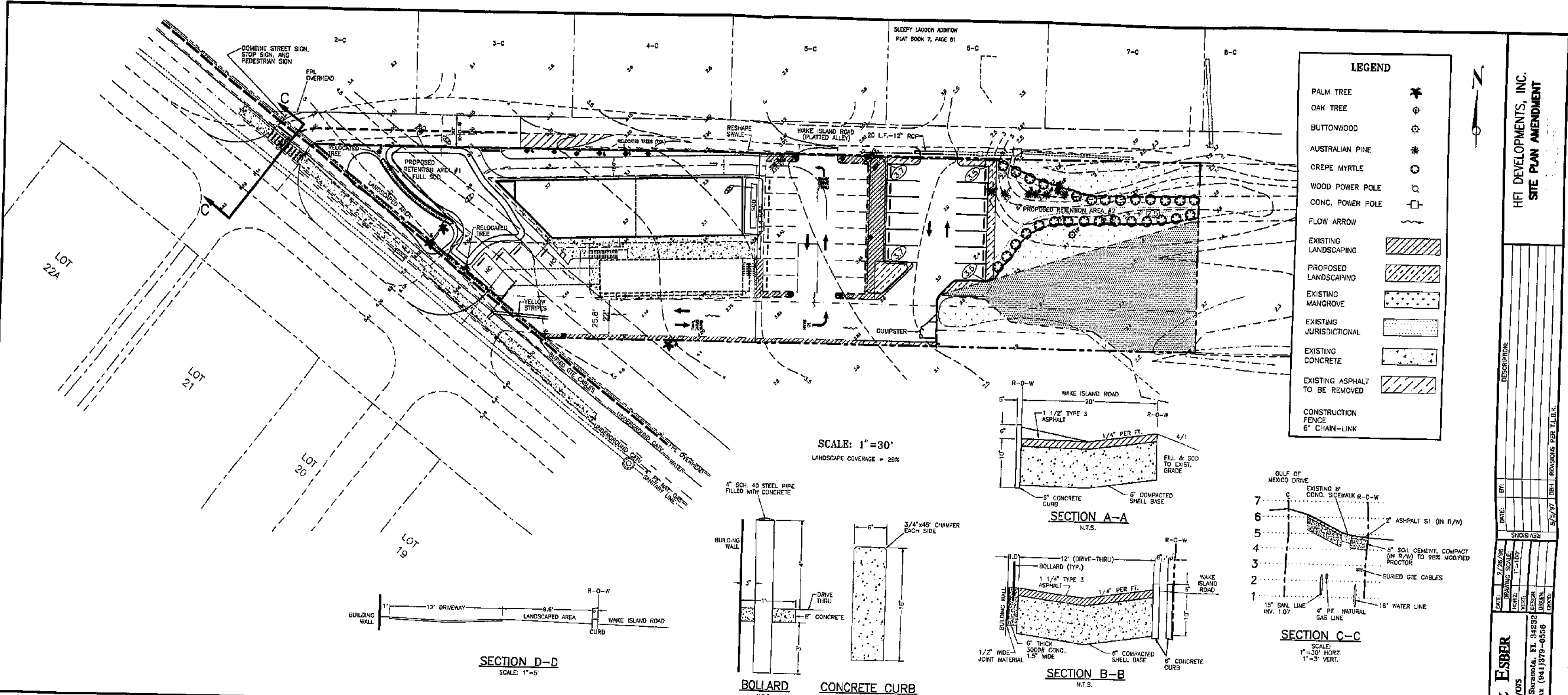
SITE PLAN AMENDMENT
AS PER PERMITS

LANDRY & ESBER
Engineers & Surveyors
1100 N. GULF DR. SUITE 100
DUNEDIN, FL 34628

D-3

**MANATEE COUNTY
FLORIDA**

SCALE	1" = 100'	SECTION	TWP.	RANGE	SHEET NO.	MAP
PHOTO DATE	NOV. 1991	23	35 S	16 E	11C-3	
PHOTO NO.	PD-3952					



HFI DEVELOPMENTS, INC.
SITE PLAN AMENDMENT

DATE:	9/23/97
BY:	DBH
DATE:	
BY:	
DATE:	
BY:	
DATE:	
BY:	
DATE:	
BY:	
DATE:	
BY:	

LANDRY & ESBER
Engineers - Surveyors
5940 Palmetto Blvd., Sarasota, FL 34232
(941) 778-8681 - Fax (941) 379-0556

PROJECT NO: B02502
SHEET NO: D-4

EXHIBIT A-4
HFI DEVELOPMENTS, INC.
SITE PLAN CHECKLIST
SECTION 158.097
"APPLICATION FOR SITE PLAN APPROVAL"

- (A) The character of the site will be an existing office building with two (2) accessory use dwelling units (apartments) on the second floor (3,500 SF) and a proposed addition with 5,532.6 SF. The proposed addition will have a 12' wide, asphalt drive-thru lane adjacent to the north side of the addition. The drive-thru lane will exit onto Wake Island Road. The section of Wake Island Road from this exit point to Gulf of Mexico Drive will be rebuilt with an asphalt travel surface.
- (B) The site plan amendment for the project has been submitted as Sheet D-4 of Exhibit B-1 (attached). The site plan labels the title of the project as HFI Developments, Inc. The existing conditions plan (D-2), has contours at one-half foot intervals referenced to mean sea level. The following chart indicates additional information found on the site plan:
- | | Proposed | Allowed |
|---|-------------------|---------------------|
| Impervious Area (S.F.): | | |
| Building Coverage | 9,339 SF | |
| Existing Parking | 5,850 SF | |
| Proposed Parking | 4,821 SF | |
| Driveway, Drive-thru and Front Parking | 10,911 SF | |
| | 30,521 SF (54.6%) | 45,302 (Max.) (80%) |
| Open Space (S.F.): | | |
| Total Site | 56,628 SF | |
| Total Impervious | 10,921 SF | |
| | 25,707 SF (45.4%) | 11,326 (Min.) (20%) |
| Front Setback (Ft.) (Existing Building) | 41.0' (Min.) | 45' (Min.) |
| Front Setback (Ft.) (Building Addition) | 43.0' (Min.) | 45' (Min.) |
| Rear Setback (Ft.) | 281.0' | 50' (Min.) |
| Side Setback (Ft.) (Existing Building) | 25.8' (Min.) | 25' (Min.) |
| Side Setback (Ft.) (Building Addition) | 15.0' (Min.) | 0' (Min.) |
| Building Separation * | N/A | 30' (Min.) |
| MHWL Setback (Ft.) | N/A | 20' (Min.) |
- * Proposed Building Addition.

- (C) An affidavit of ownership is included.
- (D) The flood zone for the existing office building with two (2) accessory use dwelling units (apartments) and one proposed addition, that is being used is A-13, EL 10.0' as shown on FIRM Map Panel 125126-0025D (May 18, 1992). A surface water management plan based on best management practices has been provided on Sheet D-4 of Exhibit B-1 (attached). The surface water management plan consists of swales and a retention pond which provide for water quality, retention requirements is provided in the adjacent wetlands. The finished floor elevation will be 5.0' and the building addition will be flood proofed to elevation 10.0'.
- (E) A soil erosion and sedimentation plan has been provided on Sheet D-4 of Exhibit B-1 (attached). Silt barriers during construction are shown.
- (F) A copy of the SWFWMD Permit 4614149.00 is on file, water and sewer service is existing. FDOT Permit 96-A-194-0017 is on file at the Town.
- | Exhibit | Permit | Agency |
|---------|--|---------------------|
| B-2 | Wastewater | FDEP (Not Required) |
| | Potable Water | FDEP (Not Required) |
| B-3 | Storm Water Modification | SWFWMD |
| B-4 | Driveway Connection for Wake Island Road | FDOT |
- Water and sewer services only are required; no permit is required for services. FDOT Connection Permit 96-A-194-0017 has been issued for the Wedebrook Rocky entrance.
- (G) There are no additional data, maps, plans or statements necessary for the particular use and activity involved.
- (H) There is no additional data pertinent to the site development plan.
- (I) We are unaware of any additional material or information that the Town may require.
- (J) Departures: (none required)
- (K) This project is not a PUD.

- (D) We have shown on Sheet D-3 and D-4, Exhibit B-1 (attached), the relationship of the project to existing development in the area including streets, utilities, residential and commercial development, ecological features and important physical features in and adjoining this project.
- (E) The proposed land use for the site is an existing office building with two (2) accessory use dwelling units (apartments) on the second floor and a proposed one story addition.
- | Existing Building | 1st Floor | 2nd Floor | 2 Apartments |
|-------------------|-------------------|-------------------|-------------------|
| | 1,800 SF (Office) | 1,980 SF | |
| Proposed Building | 1 Floor | 1,100 SF (Retail) | 3,818 SF (Office) |
- (F) We have provided the following tabulation of upland and wetland acreage and percentages:
- | | Acres | Percent |
|---|--------------------------|---------|
| (1.) Jurisdictional, Landward of MHWL: | 0.21 | 16.1 |
| Uplands: | 1.09 | 83.8 |
| Total Project: | 1.30 | 100.0 |
| (2.) Total Site: | 1.3 Acres (56,628 SF) | |
| Impervious: | -.71 Acres (30,921 SF) | |
| Open Space: | .59 Acres (25,707 SF) | |
| Total Open Space % = 25,707 / 56,628 = 45.4% | | |
| Provided 45.4%, Minimum Required 20% | | |
| (3.) Total Site: | 1.3 Acres (56,628 SF) | |
| Jurisdictional Lands: | .21 Acres (-) (9,148 SF) | |
| Impervious: | .71 Acres (30,921 SF) | |
| Upland Open Space: | = 16,559 SF | |
| Total Upland Open Space % = 16,559 / 56,628 = 29.2% | | |
| Provided 29.2%, Minimum Required 15% | | |
- (G) We have provided the architectural definition of the building envelope for the accessory use structures in Exhibit B-1 (attached).
- (H) The type and location of all trees protected by Town regulation have been shown on Sheet D-6 of Exhibit B-1 (attached). All trees in the proposed parking area have been removed under Permit TP 00207 issued 7/28/95.
- (I) We have shown the location of all the existing utilities on Sheet D-4 of Exhibit B-1 (attached).

- (J) The landscape features of the project have been shown on the Sheet D-4 as part of Exhibit B-1 (attached). The existing landscape area per SPE 94-64 is shown on Sheet D-4 with the types and sizes of plantings. These plantings are along the south property line to the east end of the existing parking area, and along the east side of the existing parking area. Existing landscaping was also installed in the existing parking area and the north side of the existing parking area. A row of 8-10 ft. oak trees were installed along the north property line and a hedge and landscaped area along the west property line was installed for the future (new proposed) parking area.
- (K) We have shown the location of all pedestrian walks on Sheet D-3 of Exhibit B-1 (attached). Walkways are along the south side of the existing building, in the concourse and along the east and west sides of the building.
- (L) We have included the location of all facilities on Sheet D-4 of Exhibit B-1 (attached). The disposition of all open space has been shown on this drawing. We have also included calculations indicating how the Town's open space requirements are being met. No recreation space is required. A list of these calculations is provided as follows:
- | | |
|---------------------|------------|
| Open Space: | 0.59 Acres |
| Total Project Site: | 1.30 Acres |
| % Open Space = | 45.4% |
- (M) The waste disposal area is existing and shown on Sheet D-4 of Exhibit B-1 (attached), all other storage areas are totally enclosed. Asphalt with 6' opaque fence.
- (N) A traffic analysis is not required. We have provided the location and dimension of the existing driveways on Gulf of Mexico Drive, on Sheet D-4 of Exhibit B-1 (attached).
- | | Office | Residential |
|-------------------|-----------------------------|----------------|
| Existing Building | 1,800 SF @ 1/250 SF = 7.2 | 2 Apartments |
| Proposed Retail | 1,100 SF @ 1/200 SF = 5.5 | |
| Proposed Office | 3,818 SF @ 1/250 SF = 15.27 | |
| | 27.97 | 2 Apartments |
| | | 2 (2 EACH) = 4 |
- Parking Required:
27.97 + 4 = 31.97 = 32 Spaces
Parking Spaces Provided = 32 (Including 2 handicap)
Loading Area = 1

DEVELOPER

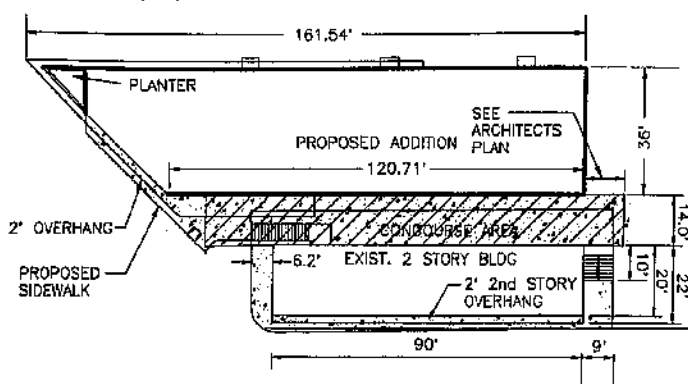
HFI DEVELOPMENTS, INC.
6350 GULF OF MEXICO DRIVE
LONGBAY KEY, FL 34228
PHONE (941) 383-8843

ENGINEER/SURVEYOR

LANDRY & ESBER
5940 PALMER BOULEVARD
SARASOTA, FL 34232
PHONE (941) 379-8831

ARCHITECT

SKIRBALL GROUP
1509 S. ORANGE AVENUE
SARASOTA, FL 34239
PHONE (941) 951-1777



BUILDING COVER
(INCLUDING SIDEWALKS)

PROPOSED ADDITION =	5532.6 s.f.
EXISTING BUILDING =	2314.4 s.f.
CONCOURSE =	1492.4 s.f.
	9339.4 s.f.
LOT COVERAGE =	BUILDING COVERAGE/TOTAL LOT AREA
LOT COVERAGE =	9339.4 s.f./56,628.0 s.f.
LOT COVERAGE =	16.49%
ALLOWABLE =	30%

HFI DEVELOPMENTS, INC.
SITE PLAN AMENDMENT
SITE DATA

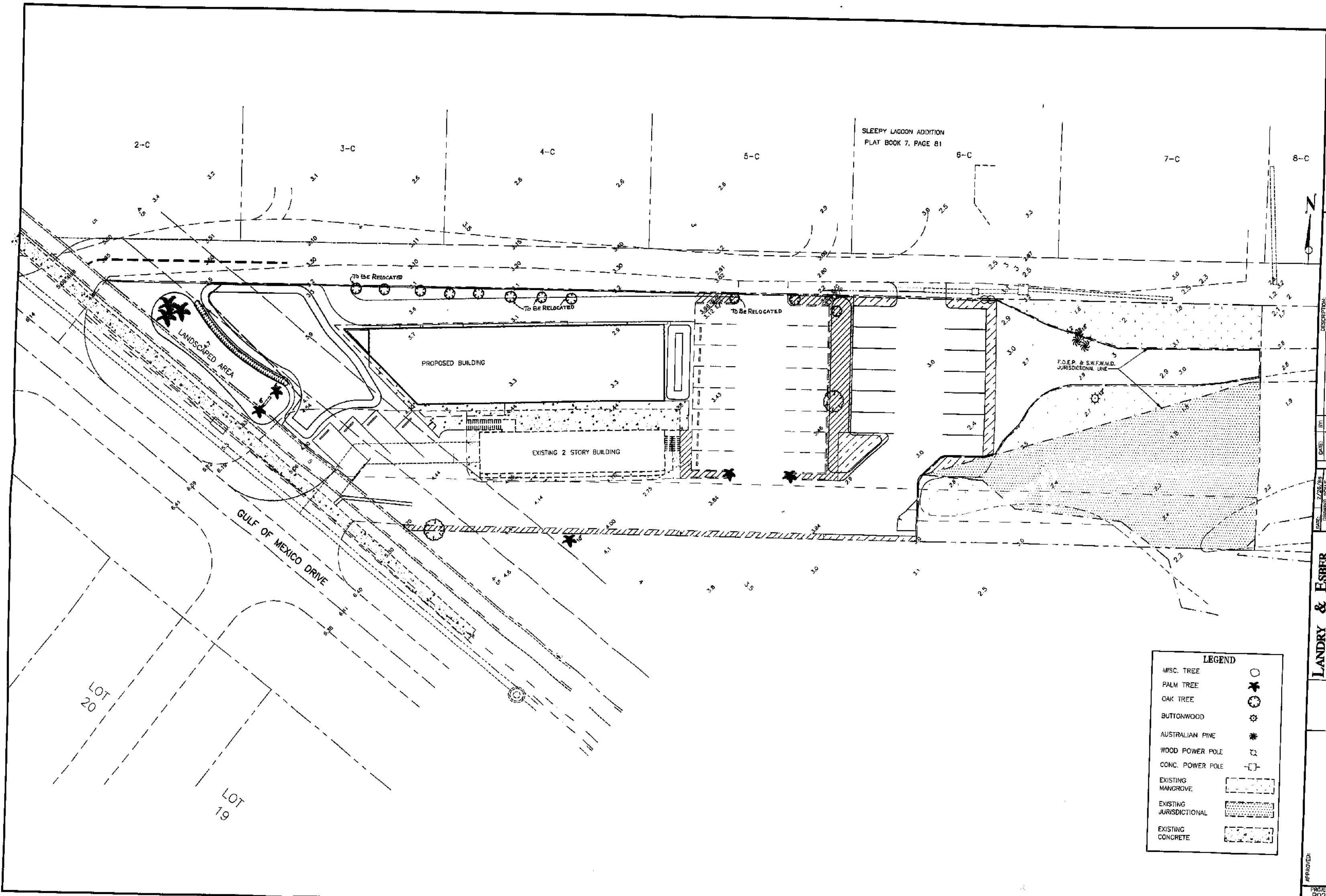
DATE	BY	DATE	BY

LANDRY & ESBER
Engineers & Surveyors
5940 Palmer Blvd. - Sarasota, FL 34232
(941) 379-0031 - Fax (941) 379-0656

APPROVED: _____ DATE _____
ENGINEER OF RECORD
NOT ISSUED FOR CONSTRUCTION

PROJECT NO:
802502

SHEET NO:
D-5



LEGEND

MISC. TREE	○
PALM TREE	✱
OAK TREE	⊙
BUTTONWOOD	⊗
AUSTRALIAN PINE	⊛
WOOD POWER POLE	⊙
CONC. POWER POLE	⊞
EXISTING MANGROVE	▨
EXISTING JURISDICTIONAL	▤
EXISTING CONCRETE	▧

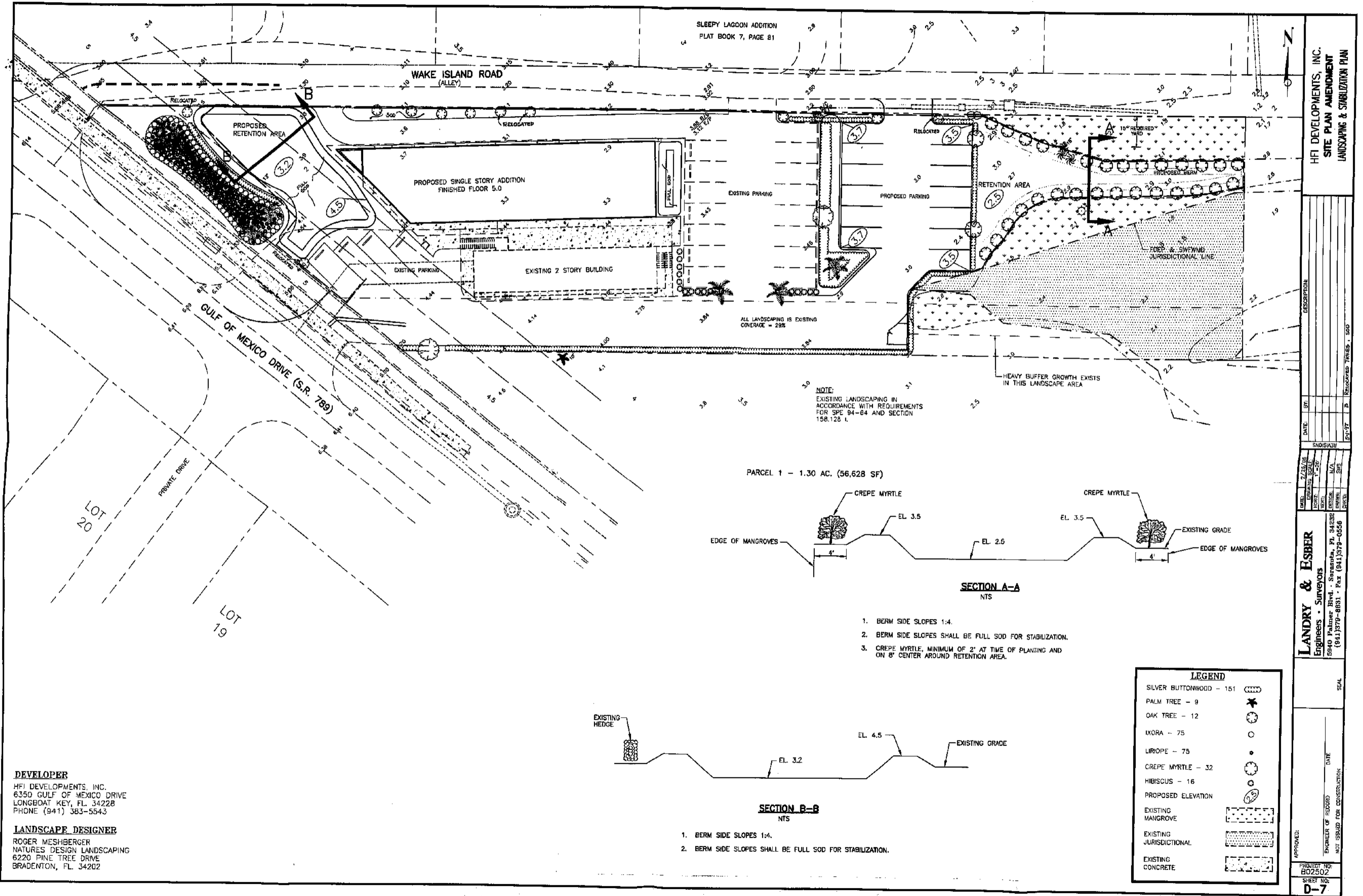
HFI DEVELOPMENTS, INC.
SITE PLAN AMENDMENT
TREE LOCATION PLAN

DATE	BY	DESCRIPTION

LANDRY & ESBER
Engineers - Surveyors
5940 Palomar Blvd. - Sarasota, FL 34235
(941) 579-8831 • Fax (941) 579-0808

APPROVED: _____
ENGINEER OF RECORD DATE _____
NOT ISSUED FOR CONSTRUCTION

PROJECT NO: B02502
SHEET NO: D-6



SLEEPY LAGOON ADDITION
PLAT BOOK 7, PAGE 81

WAKE ISLAND ROAD
(ALLEY)

PROPOSED
RETENTION AREA

PROPOSED SINGLE STORY ADDITION
FINISHED FLOOR 5.0

EXISTING PARKING

PROPOSED PARKING

RETENTION AREA

EXISTING PARKING

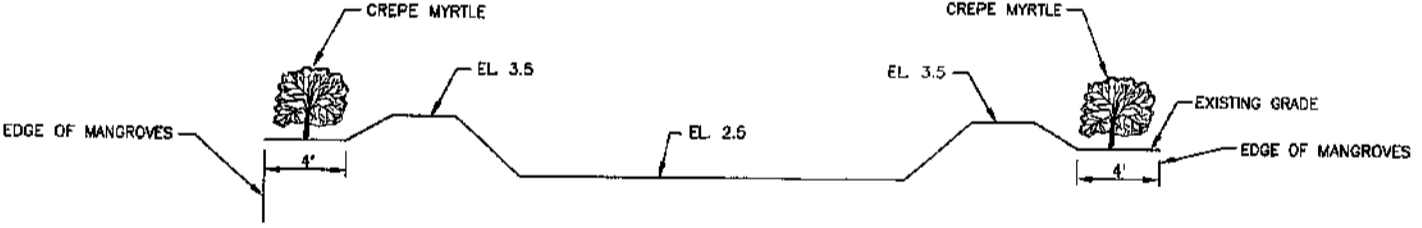
EXISTING 2 STORY BUILDING

ALL LANDSCAPING IS EXISTING
COVERAGE = 29%

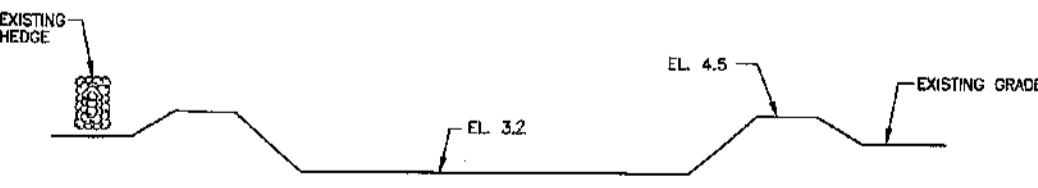
HEAVY BUFFER GROWTH EXISTS
IN THIS LANDSCAPE AREA

NOTE:
EXISTING LANDSCAPING IN
ACCORDANCE WITH REQUIREMENTS
FOR SPE 94-84 AND SECTION
158.128 L

PARCEL 1 - 1.30 AC. (56,628 SF)



1. BERM SIDE SLOPES 1:4.
2. BERM SIDE SLOPES SHALL BE FULL SOD FOR STABILIZATION.
3. CREPE MYRTLE, MINIMUM OF 2' AT TIME OF PLANTING AND ON 8' CENTER AROUND RETENTION AREA.



1. BERM SIDE SLOPES 1:4.
2. BERM SIDE SLOPES SHALL BE FULL SOD FOR STABILIZATION.

LEGEND

SILVER BUTTONWOOD - 151	(Symbol)
PALM TREE - 9	(Symbol)
OAK TREE - 12	(Symbol)
IKORA - 75	(Symbol)
LIRIOPE - 75	(Symbol)
CREPE MYRTLE - 32	(Symbol)
HIBISCUS - 16	(Symbol)
PROPOSED ELEVATION	(Symbol)
EXISTING MANGROVE	(Symbol)
EXISTING JURISDICTIONAL	(Symbol)
EXISTING CONCRETE	(Symbol)

DEVELOPER
HFI DEVELOPMENTS, INC.
6350 GULF OF MEXICO DRIVE
LONGBOAT KEY, FL 34228
PHONE (941) 383-5543

LANDSCAPE DESIGNER
ROGER MESHBERGER
NATURES DESIGN LANDSCAPING
6220 PINE TREE DRIVE
BRADENTON, FL 34202

HFI DEVELOPMENTS, INC.
SITE PLAN AMENDMENT
LANDSCAPING & STABILIZATION PLAN

DATE	BY	DESCRIPTION

LANDRY & ESBER
Engineers - Surveyors
8940 Palmer Blvd. - Sarasota, FL 94183
(941) 570-8831 - Fax (941) 379-0556

APPROVED: _____
ENGINEER OF RECORD DATE _____
NOT ISSUED FOR CONSTRUCTION

PROJECT NO:
802502

SHEET NO:
D-7